

CITY OF VACAVILLE

RESIDENTIAL DESIGN
REQUIREMENTS
FOR NEW
SINGLE FAMILY
DEVELOPMENT

6/11/96

TABLE OF CONTENTS

	<u>Page No.</u>
I. INTRODUCTION	1
A. PURPOSE	1
B. APPLICATION OF THE REQUIREMENTS TO NEW SINGLE FAMILY DEVELOPMENT	2
C. REVIEW PROCESS	3
D. ADMINISTRATION	3
II. SUBDIVISION DESIGN	5
A. ORDINANCE REQUIREMENTS	5
1. <i>Calculation of Dwelling Unit Density</i>	5
2. <i>Street Parking</i>	6
3. <i>Street Design</i>	6
4. <i>Sidewalks</i>	6
5. <i>Perimeter Fencing</i>	7
6. <i>Common Area Landscaping</i>	8
7. <i>Perimeter Landscaping</i>	8
8. <i>Tree Preservation</i>	9
9. <i>Centralized Mailbox</i>	10
10. <i>Performance Standards</i>	10
11. <i>Alternative Configuration For Single Family Dwellings</i>	13
12. <i>Infill Projects</i>	14
13. <i>Housing Mix</i>	16
14. <i>Subdivision Signs</i>	16
B. DESIGN STANDARDS	17
1. <i>Distribution of House Plans</i>	17
2. <i>House Plans on Adjoining Lots</i>	17
3. <i>Carriage Units</i>	17
4. <i>Alleys</i>	18
5. <i>Private Roadways</i>	19
6. <i>Parking Standards for Developments with Private Roadways</i>	20
7. <i>Common Driveways</i>	20
8. <i>Recreation/Open Space for Projects with a Minimum Lot Size of 4,500 sq. ft.</i>	21
9. <i>Number of House Designs in a Subdivision</i>	23
10. <i>Neighborhood Identity</i>	23
11. <i>Mix of One and Two-Story Plans</i>	24
12. <i>Mix of House Plan Designs</i>	24

	<i>Page No.</i>
(SUBDIVISION DESIGN STANDARDS CONTINUED)	
13. <i>Pedestrian and Bicycle Access</i>	24
14. <i>Open Space</i>	24
15. <i>Standards for Model Homes and Complexes</i>	24
16. <i>CC&R's Required</i>	25
C. GUIDELINES	25
1. <i>One and Two-Story Plans in a Row</i>	25
2. <i>Duplexes on Corner Lots</i>	26
3. <i>Theme Street Lights</i>	26
4. <i>Curvilinear Street Pattern</i>	26
5. <i>Side-On Cul-De-Sacs</i>	26
6. <i>Variation of Lot Width</i>	26
7. <i>Recycling Areas</i>	26
8. <i>Trip Reduction Features</i>	27
9. <i>Energy Efficient Design</i>	27
III. SITE PLAN DESIGN (FOR INDIVIDUAL LOTS)	28
A. ORDINANCE REQUIREMENTS	28
1. <i>Compliance with Development Standards</i>	28
2. <i>Off-Street Parking</i>	29
3. <i>Front Setback Variation</i>	29
4. <i>Front and Corner Lot Street Side Yard Landscaping</i>	30
5. <i>Maximum Paved Area</i>	31
6. <i>Permanent Foundation</i>	31
7. <i>Lot Coverage</i>	31
8. <i>Setback Exceptions</i>	31
9. <i>Home Safety and Security</i>	32
10. <i>Driveways</i>	32
11. <i>Fencing</i>	32
B. DESIGN STANDARDS	33
1. <i>Driveways</i>	33
2. <i>Floor Area Ratio</i>	34
3. <i>Street Trees</i>	34
4. <i>Minimum Eave Overhang</i>	34
IV. ARCHITECTURAL DESIGN	35
A. ORDINANCE REQUIREMENTS	35
1. <i>Design Review Required</i>	35
2. <i>Screening of Mechanical Equipment</i>	35

(ARCHITECTURAL DESIGN CONTINUED)		<u>Page No.</u>
B.	DESIGN STANDARDS	35
1.	<i>Style of Architecture</i>	35
2.	<i>Wall Planes</i>	36
3.	<i>Siding Materials</i>	36
4.	<i>Materials and Features on Side and Rear Elevations</i>	37
5.	<i>Exterior Colors</i>	37
6.	<i>Roofing</i>	37
7.	<i>Windows/Vents</i>	38
8.	<i>Window/Private Open Space Orientation</i>	38
9.	<i>Garage Design</i>	38
10.	<i>Entry Treatment</i>	39
11.	<i>Security Design</i>	40

LIST OF FIGURES

<i>Figure 1.</i>	<i>Changes in Plane for Soundwalls</i>	8
<i>Figure 2.</i>	<i>Recommended Tree Preservation Measures</i>	9
<i>Figure 3.</i>	<i>Example of Front Yard Setback Variation</i>	28
<i>Figure 4.</i>	<i>Examples of Trim Elements</i>	35
<i>Figure 5.</i>	<i>Recommended Change in Roof Line</i>	37
<i>Figure 6.</i>	<i>Recommended Decrease in Width of Garage</i>	38
<i>Figure 7.</i>	<i>Recommended Entry Treatment</i>	39

APPENDICES

<i>Appendix 1.</i>	<i>Residential District Development Standards</i>	40
<i>Appendix 2.</i>	<i>Alternative Configurations for Single Family Dwellings</i>	42

I. INTRODUCTION

A. PURPOSE

The Residential Design Requirements for New Single Family Development is a companion document to Chapter 14.074 of the Land Use and Development Code, Residential Development Standards. These Requirements are a compilation of ordinances, standards, and design guidelines that implement the Code provisions.

The purpose of this document is to guide developers, decision-makers, designers, architects, engineers, and staff in the development and review of new single family residential projects. It is the City's objective, in developing these Requirements, to provide a comprehensive set of standards for single family residential development that is easy to use. Another goal is to include adequate flexibility to encourage innovative and creative design solutions. Finally, the Requirements are designed to ensure the development of high quality residential subdivisions in the City of Vacaville.

B. APPLICATION OF THE RESIDENTIAL DESIGN REQUIREMENTS TO NEW SINGLE FAMILY DEVELOPMENT

The City's expectations of project quality and design are established through its adopted ordinances, standards, and guidelines. Section 14.003.020.C of Division I, Administration, authorizes the City Council to adopt design guidelines as a supplement to the regulations of Title 14, Land Use and Development. Compliance with the Design Requirements shall be as follows:

- 1. ORDINANCE.** *Compliance with the provision of an **ordinance** is required unless the provision is specifically waived, substituted, or otherwise altered through the approval of a variance, an administrative clearance, or a planned development (PD). The text of the Land Use and Development Code has been summarized in order to make these regulations more user friendly. The text of the Code should be consulted, however, for the technical detail of the ordinance requirements.*
- 2. DESIGN STANDARD.** *Compliance with the design **standards** is required in conjunction with the approval of a Planned Development or Design Review unless the decision-maker approves an alternative provision as part of a project approval.*
- 3. GUIDELINE.** *Compliance with **guidelines** is encouraged, but can be required through project conditions of approval. Compliance with guidelines may also be considered by the decision-maker(s) as a component to support an increase in density above the base of the applicable density range in accordance with Chapter 14.074, Residential District Development Standards.*

The Residential Design Requirements are minimum standards and shall not be construed to prevent the City Council, the Planning Commission, or the Director from imposing, as part of a project approval, specific conditions, which may be more restrictive, in order to meet the intent of the Residential District Development Standards.

These Requirements apply to all residential land development permits and approvals that are subject to the Zoning Ordinance and the Subdivision Ordinance, and are applicable to new construction or to the modification of existing structures. Such new construction or modifications shall be subject to the development standards contained in Chapter 14.074 of the Land Use and Development Code, Residential Development Standards, unless otherwise specified in the applicable zoning district, or in Chapter 17.74, Non-Conforming Uses, of the Zoning Ordinance.

Some of the ordinances, standards, or guidelines may relate only to production homes and may not be applicable to custom home subdivisions.

Where these Requirements conflict with any deed restriction, covenant, easement, or other legal written agreement, the provisions of the Residential Design Requirements shall prevail for the purposes of city review. The Requirements were designed to supplement the Development Code and Policy Plan/Specific Plan standards; however, where they are inconsistent, the regulations adopted in these Codes and Plans shall apply.

C. REVIEW PROCESS

Design review or the consideration of a planned development is required for each residential development. These guidelines shall be used by the City in evaluating residential project proposals as part of the development review process. Application and review procedures for residential projects are set forth in the Development Code and submittal requirements. In general, all design review submittals should include a master plotting or site plan and elevations and floor plans for the models.

D. ADMINISTRATION

The provisions of these Requirements shall be administered by the Director of Community Development in accordance with the following standards and with the provisions of the City of Vacaville Land Use and Development Code, the California Public Resources Code, and all other applicable state and local regulations. If ordinances, laws, or regulations related to single family residential development are adopted or amended, then these Requirements shall be construed to include such additions or amendments, and no modification of these Requirements shall be necessary.

The Requirements, including all graphics, may be updated administratively to reflect new ordinance requirements or amendments to the Municipal Code. It shall be the intent of the City to update this document as often as practical in order to provide interested parties with an accurate and complete set of regulations. New standards and guidelines, which are not mandated through changes or additions to other regulations, shall be processed in accordance with Chapter 14.001 of the Land Use and Development Code, Amendment to the Development Code. In summary, the Planning Commission shall hold a public hearing and make recommendations to the City Council on proposals to amend the Residential Design Requirements. The City Council shall adopt any amendments to the Requirements by resolution.

*Terms referenced in this document are also defined in the Land Use and Development Code. **Decision-maker** means the person or body with the authority to approve a project in the City of Vacaville, as authorized in Division I, Administration, of Title 14. Director of Community Development means the Director of Community Development of the City of Vacaville or a person designated by the Director to assume some or all of the Director's duties. For the purposes of these Requirements, any reference to **Director** shall mean the Director of Community Development unless specifically noted otherwise.*

These Requirements are organized by the major categories of subdivision design, site plan design (for individual lots), and architectural design. Subdivision design relates to street improvements, dwelling unit placement, and common facilities for the overall site development. Site plan design applies to placement of homes on individual lots. Architectural design pertains to the exterior building features of residential dwelling units. Within each major category, the Requirements are grouped by ordinance, standards, and guidelines, as defined above. A reference to the appropriate section of the Municipal Code or implementing policy is provided at the end of each ordinance, standard, or guideline (will be provided prior to next draft). Some of these Requirements are noted as ordinances, although they have not yet been adopted or codified. They have been listed this way because it is anticipated that adoption of the implementing ordinances will occur within the next few months.

II. SUBDIVISION DESIGN

A. ORDINANCE REQUIREMENTS

1. **CALCULATION OF DWELLING UNIT DENSITY.** *The density ranges in the General Plan establish the potential number of dwelling units per gross developable acre allowed within each residential zoning district. The base density is the lowest point of the General Plan density range. Density of a proposed project is calculated by dividing the number of proposed units by the gross developable acres of the project site. Gross developable area of a project is defined in Division II, Definitions, of Title 14 and shall be established through a planned development, a specific plan, or policy plan in accordance with Section 14.074.040 of Title 14. If the number of units calculated on the project site includes a remainder of 90 percent or more, one unit may be added to the total. (Reference Section No. 14.074.040)*
 - a. *Increase Above the Base Density. A project may be approved with an incremental increase above the base density, but within the General Plan range, when the project as proposed includes, either on-site or off-site components which are found by the decision-maker to provide community-wide benefit. Such components shall include, but not be limited to, at least one of the following: housing that meets the remaining unmet projected need of households in the very low, low, and moderate income categories; superior site design or architectural quality; preservation of significant natural resources beyond the Code requirements; increased public amenities such as parks or open space above the Code requirements and which are not subject to compensation by the City; increased private recreation and open space greater than the requirements of the Code; and other*

improvements that provide public benefit and are substantially in excess of the Code requirements.

b. *Decrease Below the Base Density.*

(1) *A residential project in a residential medium or high density district may be approved with a density of no more than 25 percent below the base if the project complies with the provisions of Chapter 14.111, Planned Developments. In a residential medium density district, the proposed project shall include a housing type other than single family, detached housing with an attached garage which fronts on a public street. In a high density district, the project shall consist of an attached housing type that is in character with the purposes of the multi-family district.*

(2) *A project with a density of no more than 10 percent below the base density may be approved in any residential zoning district if the decision-maker determines that significant or unusual site constraints, such as site configuration, natural features, or dedications of right-of-way, prevent a project from attaining the base density.*

2. **STREET AND/OR COMMON PARKING.** *No less than one street parking space, located outside the driveway area, shall be provided adjacent to each lot developed with a dwelling unit in all subdivisions except in RLM 3.6, RLM C (Clustered) and RM Districts and in projects with private streets. Parking for projects with private streets shall be as determined through the planned development application. (Reference Section No. _____)*

3. **STREET DESIGN.** *Streets shall comply with all applicable standards set forth in the Municipal Code and Standard Specifications. Adequate sight distance shall be provided in the design of all streets and intersections. (Reference Section No. _____)*

4. **MEANDERING/MONOLITHIC SIDEWALKS.** *Sidewalks shall be provided on both sides of a public street. For arterial streets, sidewalks shall be five feet in width, separated from the curb, and may be required to meander (horizontal) or undulate (vertical) to provide visual interest. Within subdivisions on local streets, sidewalks shall be five feet wide; monolithic sidewalks are permitted, but detached parallel sidewalks may be permitted. Special consideration shall be given to the design and placement of meandering and*

detached sidewalks to provide adequate drainage and reduce maintenance costs. (Reference Section No. _____)

- 5. PERIMETER FENCING/SOUNDWALLS.** *Decorative masonry walls and other types of fencing shall be required along the perimeter of residential projects to protect residents from noise and other impacts created by more intensive adjoining land uses. All fencing or walls backing up to an arterial or collector street, as identified in the General Plan, or required as part of a land use approval or a new subdivision, shall be subject to design review. Fencing required by the Code shall be installed in conjunction with any development project. (Reference Section No. 14.074.120)*
- a. *Decorative masonry walls a minimum of 6 feet in height are required in the following situations: when the site is adjacent to a multi-family residential district; when the site is adjacent to a park, school, or other non-residential use, except when adjacent to natural open space areas, including creekways; and when adjacent to a parking facility of four or more spaces. All required walls shall be installed in conjunction with the residential development project.*
 - b. *In addition, a decorative masonry wall is required for noise mitigation, as determined through an acoustical analysis. The height of a masonry wall required for the mitigation of noise shall be determined by the findings of an acoustical study, but shall be no less than 6 feet in height. Wall height shall be measured from the finished grade level established through a subdivision improvement plan or approved grading plan, on the side on which the height is greatest. Berming shall be used in conjunction with a wall when an acoustical study has recommended that fencing over eight feet in height be installed. The wall height shall be sufficient to mitigate noise within the subdivision to a maximum exterior noise level of 60 dBA DNL. An acoustical analysis prepared by an acoustical engineer shall be submitted to the Director. The acoustical analysis shall address the necessary wall height based on buildout traffic levels as contained in the most recent traffic studies.*
 - c. *Walls installed as part of back-up treatment along a freeway, railroad right-of-way, or arterial shall be installed in accordance with the provisions of Division XII, Dedications and Improvements, of the Land Use and Development Code.*
 - d. *The wall shall angle at the corners of street intersections to provide a larger entry landscape treatment. The back-up wall shall not be a flat*

plane, but shall meander or include offsets to be approximately 15 to 20 feet behind face of curb.

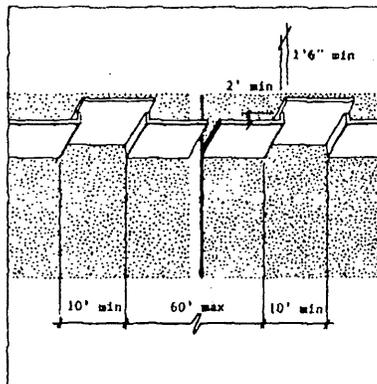


Figure 1. Recommended changes in plane for soundwalls. Alternatives could also include angled walls or other variations

- e. Walls should be constructed with durable and decorative materials such as split face or scored masonry block or brick. Panel or precast walls will only be permitted if they are of high quality materials and design. Walls should also include vines or be constructed with materials that discourage graffiti.
 - f. All perimeter walls should include offsets and breaks to provide visual interest to the streetscape.
 - g. In larger lot subdivisions, open style fencing, such as wrought iron, should be installed when a block wall is not required. Along open space areas such as creekways and bike paths, more durable double-sided wood fencing is required. Along community facilities such as parks, a decorative, sound attenuating masonry wall shall be provided. Special Caltrans specification fencing is required surrounding hillside open spaces which are used for cattle grazing.
6. **COMMON AREA LANDSCAPING.** Landscaping shall be provided for all common areas within single family residential projects and shall be installed in compliance with the requirements contained in the Water Efficient Landscape Regulations and other regulations, as applicable. (Reference Section No. 14.074.130)
7. **PERIMETER LANDSCAPING.** All back-up areas between the curb and perimeter walls shall be fully improved with low-maintenance, irrigated landscape treatment which may include trees, shrubs, natural ground cover,

rock, or other decorative paving materials. Landscape plans shall provide adequate sight distance on all streets and at all intersections. Plans shall be approved by the Director, City Engineer, and City Landscape Architect prior to recordation of the Final Map. (Reference Section No. _____)

- a. Streetscape landscaping adjacent to arterial streets should emphasize high quality design. Plant materials and design should be compatible with the adjacent neighborhood(s). Berming and variation in plant materials is also encouraged. All landscaping is subject to the requirements contained in the City of Vacaville Water Efficient Landscape Regulations. Street trees should provide shade and emphasize a particular design theme. Acceptable street tree species are listed in the Water Efficient Landscape Regulations.
- b. All landscaping and berming should be designed and located to provide adequate surveillance of perimeter fence areas by police.

- 8. TREE PRESERVATION.** Existing trees, including but not limited to oaks and black walnuts, shall be incorporated into the residential subdivision. The subdivision shall be designed so that these trees are located within common or public areas and benefit the entire neighborhood and surrounding community. If located in residential yard areas, the development CC&R's shall include measures for the maintenance and preservation of the trees. The developer shall submit to the Director specified tree preservation measures for the existing trees to be retained. The tree preservation plan shall be prepared by a certified arborist and submitted in conjunction with the development proposal. No tree removals for the purpose of developing a subdivision shall be performed without authorization by the Director or through approval of the development by the decision-maker.

Protective fencing shall be placed around the drip line of all trees preserved in the development. No utilities shall be permitted within the drip line. The original grade within the drip line shall be preserved and a drainage outlet provided if necessary. Landscape treatment within the drip line may vary depending on the tree species and existing conditions, soil type, and other site conditions.

(Reference Section No. _____)

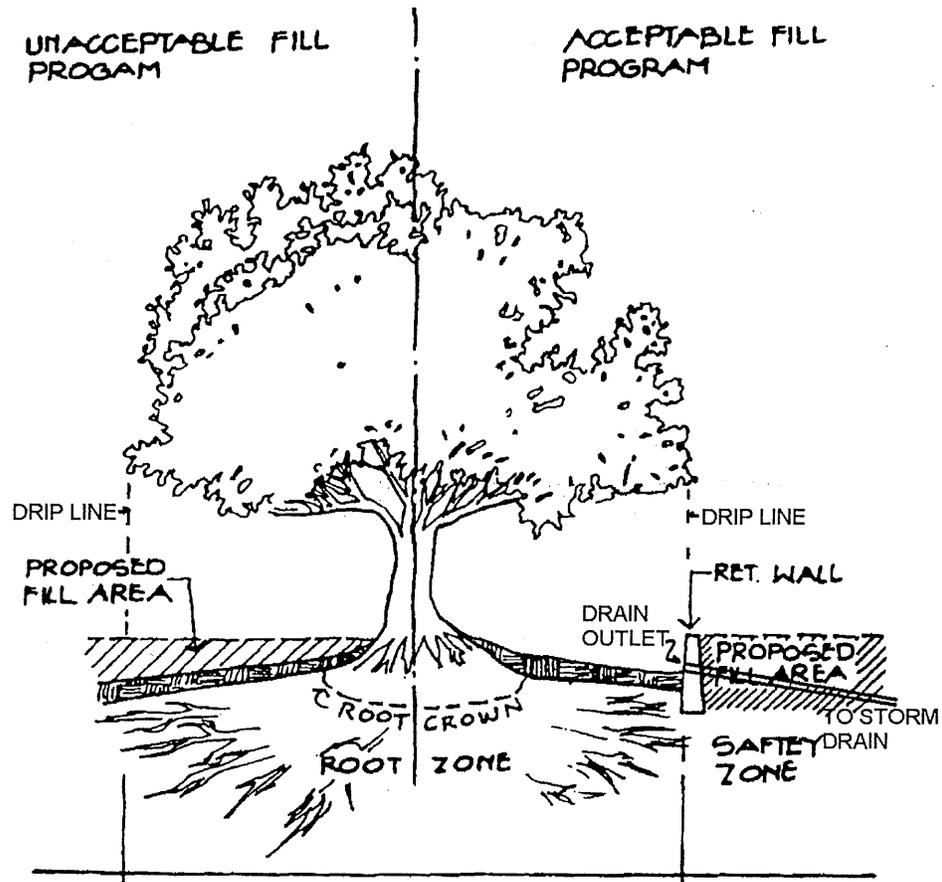


Figure 2. Recommended tree preservation measures

9. **CENTRALIZED MAILBOX.** Development plans shall make provisions for the placement of a centralized mailbox delivery and/or parcel locker units. The developer should consult with the Postal Service on the design and location of these mailbox units. A separate drawing indicating the location of the units shall be submitted with the subdivision improvement plans for review and approval by the City. (Reference Section No. _____)
10. **PERFORMANCE STANDARDS.** Single family residential development shall comply with the Performance Standards as set forth in Chapter 14.127 of the Land Use Development Code and as summarized below. (Reference Section No. _____)
 - a. **Hours of Construction.** No construction equipment shall be operated nor any outdoor construction or repair work shall be permitted within 500 feet from any occupied residence during the hours of 10:00 p.m. to 6 a.m. or until 8 a.m. on Sunday mornings. Interior work which

would not create noise or disturbance noticeable to a reasonable person of normal sensitivity in the surrounding neighborhood shall not be subject to this standard. A request for an exception to the permitted construction hours and days may be granted by the Director for emergency work, to offset project delays due to inclement weather, for 24-hour construction projects, or other similar occurrences.

- b. *Parking Lot Lighting.* Lighting shall be provided for all parking areas that are not located on a residential lot in compliance with the *Off-Street Parking and Loading Design Guidelines*. Lighting shall be shielded and directed so as to not create a hazard or nuisance to other properties or impact traffic on adjacent streets. Lighting shall be a minimum maintained of one foot candle and a maximum of six foot candles. A photometric plan demonstrating compliance with these lighting standards and a site plan showing the location and design of lighting fixtures may be required as a condition of approval.

- c. *Noise.* The noise standards in Chapter 14.127 of the *Land Use and Development Code* apply to all residential project approvals. The standards identify the maximum noise levels to which residential land uses may be exposed. The noise level exposure of a project may be determined by an acoustical study. Preliminary determinations can be made by locating the project site on the *Vacaville Noise Contours Map* (Figure 14.127-1 in the *Land Use and Development Code*) for ground transportation and the *Vacaville Airport Noise Contours Map* (Figure 14.127-2 in the *Land Use and Development Code*). The exterior affected location typically is the backyard for single family residential uses.

An acoustical study, prepared in a format consistent with the criteria established in the *Noise Element of the General Plan*, shall be required when residents would potentially be exposed to the noise levels exceeding the standards noted below. The study shall demonstrate compliance with the noise standards and ensure that residents will not be exposed to noise levels in excess of the standards established for both transportation and non-transportation noise sources or by identifying mitigation measures that would result in compliance with such standards.

- (1) The interior noise standard for ground transportation sources of noise, including traffic on public or private roadways and railroad operations, is 45 dB DNL interior and 60 dB DNL exterior.

- (2) *As a guide for approving land use changes to residential, the following ground transportation standards apply:*
 - (a) *Exposure to noise levels between 40 and 60 dB DNL is normally acceptable with typical conditions of approval (setbacks, walls, fences and standard building practices).*
 - (b) *Exposure to noise levels between 60 and 75 dB DNL is conditionally acceptable and subject to noise study to demonstrate noise can be reduced to normally acceptable levels with acceptable mitigation.*
 - (c) *Exposure to noise levels above 75 dB DNL is normally unacceptable regardless of measures implemented to reduce noise.*

- (3) *The maximum noise exposure levels to non-transportation sources is as follows:*
 - (a) *The hourly Leq, dBA levels for exterior noise are 50 in the daytime (7 a.m. to 10 p.m.) and 45 at night (10 p.m. to 7 a.m.). The hourly Leq, dBA levels for interior noise are 45 in the daytime (7 a.m. to 10 p.m.) and 35 at night (10 p.m. to 7 a.m.).*
 - (b) *The hourly maximum levels, dBA, for exterior noise are 70 in the daytime (7 a.m. to 10 p.m.) and 65 at night (10 p.m. to 7 a.m.).*

- (4) *For airport related noise sources, including aircraft associated with the Nut Tree Airport, with Travis Air Force Base, and with other aviation activity affecting the City, the following standards apply:*
 - (a) *Exposure to noise levels below 55 dBA CNEL is clearly acceptable.*
 - (b) *Exposure to noise levels between 55 and 60 dBA CNEL is normally acceptable. Aviation easements and notice to residents shall be provided in accordance with Chapters 17.64 and 17.67 of Title 17.*

- (c) *Exposure to noise levels above 60 dBA CNEL is clearly unacceptable.*
- d. *Security Measures. Dwelling units shall be designed and constructed to provide for reasonable security and safety of residents and to minimize the demand for police and fire services, as determined by the decision-maker.*

11. ALTERNATIVE CONFIGURATION FOR SINGLE FAMILY DWELLINGS.

In order to ensure visible design diversity in residential districts, standards are established which require alternative design configurations for single family units. An alternative configuration is a design that varies significantly from the typical single family architectural style wherein an attached garage that fronts on the street is the dominant feature of the dwelling. (Reference Section No. 14.074.090)

- a. *Alternative configurations include the following:*
 - (1) *Units with a detached garage or a garage located on the rear half of the lot;*
 - (2) *Units which take access from a private court or an alley;*
 - (3) *Units with a garage door approximately perpendicular to the street on which the unit fronts;*
 - (4) *Units with a tandem, two car garage and a single car garage door;*
 - (5) *Units with a garage door located a minimum of five feet behind the front line of the habitable portion of the unit which contains the front entry to the dwelling and a covered porch;*
 - (6) *Units on wide and shallow lots where the lot width is at least 30 percent greater than the minimum width required for the residential district, where the width of the unit is proportionate to the width of the lot, and where no more than a two car garage is provided, except that that a third space may be provided in a tandem configuration;*
 - (7) *Units with other alternative configurations found by the decision-maker to be consistent with the intent of this section.*

- b. *Alternative configurations shall be provided for single family residential projects as follows:*
- (1) *In the RR, RE, RL-10, RL-8, and RL-6 districts, alternative configurations are encouraged within the mix of units;*
 - (2) *In the RL-5 district, at least 25 percent of all units shall be an alternative configuration, and shall be dispersed proportionally throughout the project;*
 - (3) *In the RLM-4.5 district, at least 50 percent of all the units shall be an alternative configuration, and shall be dispersed proportionally throughout the project.*
 - (a) *If the minimum lot width is reduced to less than 45 feet through a planned development, 100 percent of the units within the project shall be an alternative configuration;*
 - (4) *In the RLM-3.6 and the RLM-C districts, 100 percent of the units within the project shall be an alternative configuration;*
 - (5) *If, when calculating the number of units which are to be an alternative configuration in accordance with the provisions of this section, the total number of units required includes a remainder of 90 percent or more, the remainder shall be counted as one full unit.*

12. INFILL PROJECTS. *A single family residential project with a minimum lot area of less than 4,500 square feet, shall comply with the following infill standards if the project abuts directly on one or more sides, or is located across a non-arterial street from an existing single family residential subdivision with a minimum lot area greater than 4,500 square feet. (Reference Section No. 14.074.080)*

In addition, to the standards of this section, the regulations of the applicable zoning district shall apply. In the event of any conflict between the two, the greater requirements shall apply.

- a. *Building setbacks abutting the perimeter of the development project.*
- (1) *When the perimeter of a development project is across a public street from a single family residential subdivision, the*

yard requirements for the perimeter of the project adjoining the street, shall be equal to or greater than the yard requirements for the single family residential subdivision located across the street;

- (2) When the perimeter of the development project abuts existing single family residences, the yard requirement from these property lines shall be equal to or greater than the yard requirements of the abutting properties.*
- b. Landscaping. Landscaping in excess of the minimum required in the Zoning Ordinance may be required on the perimeter of the development project abutting single family development in order to screen or enhance the appearance of the project.*
- c. Vehicular Access. Access for a project shall be designed to minimize traffic impacts on existing local residential streets. To the extent feasible and when consistent with other applicable City policies and accepted traffic engineering practices, site access shall be provided through arterial streets. Access along non-arterial streets may be approved by the decision-maker if no other alternative is determined to be feasible.*
- d. Exterior Materials. The quality and type of exterior materials proposed for the project shall be compatible with or comparable to the materials of the existing abutting single family development. These include, but are not limited to, roofing, siding, windows, masonry accent materials, and architectural features, such as bay windows and porches.*
- e. Architecture of dwellings, their placement on the site, and building massing along the perimeter of the development project, shall be compatible with the abutting residences. Project characteristics to be considered when determining compatibility shall include, but not be limited to, the following:*

 - (1) Mix of one or two story dwellings;*
 - (2) Location of windows and balconies on units along the perimeter of the site;*
 - (3) Building height;*

- (4) *Lot width in comparison to existing lots where the project site abuts residences;*
- (5) *Variation in building mass for multi-family projects.*

13. HOUSING MIX. *Any residential project where the total number of units exceeds 400, or any application for a portion of a site where the total units will exceed 400, shall incorporate a mix of housing types and a mix of density ranges generally consistent with the General Plan policy requiring a mix of 60 percent single family, 20 percent moderate density, and 20 percent apartment type units. Additionally, at least 10 percent of the total number of units shall be on lots with a minimum area requirement of 8,000 square feet or greater, with an average dwelling unit area of 2,000 square feet or greater. (Reference Section No. 14.074.070)*

14. SUBDIVISION SIGNS. *Subdivision signs to advertise the development are permitted in accordance with Chapter 17.48 of Title 17 as follows: (Reference Section No. 17.48.450)*

- a. *Two off-site directional signs may be permitted for each subdivision indicating only travel directions, the name and address of the subdivision, the subdivider, the developer and the trademark or insignia of same, and the typical price range of units, not exceeding thirty-two square feet in total area for each sign nor more than ten feet in height and ten feet in horizontal length.*
- b. *Two on-site identification signs may be permitted not exceeding thirty-two square feet in total area for each sign nor more than six feet in height nor ten feet in horizontal length.*
- c. *One identification sign may be permitted per model home not exceeding eight square feet in area nor three and one-half feet in height. In addition to the above, one flag or banner is permitted per model home to identify the model home area and the sales complex area.*
- d. *Off-site subdivision signs shall not indicate the attributes of the subdivision, the financing methods or terms available or other phraseology which has no connections with directing traffic; nor shall there be any additions, tag signs, streamers or appurtenances added to such signs.*
- e. *Subdivision signs shall be required to undergo design review approval pursuant to Chapter 14.113 of Title 14. A subdivision sign permit shall*

be obtained which shall cover the subdivision's total signing program. There shall be no fee for this permit but a deposit of one hundred dollars shall be filed for each sign to assure removal of all signs within ten days of completion of the sales program within the subdivision.

B. STANDARDS

1. **DISTRIBUTION OF HOUSE PLANS.** *House plans shall be evenly distributed throughout the subdivision. Individual house plans shall be placed on lots to visually emphasize the architectural diversity of the residential development.*
2. **HOUSE PLANS ON ADJOINING LOTS.** *The same house plan shall not be placed on adjoining lots, except as approved by the Director in the following situations:*
 - a. *On cul-de-sacs, if the adjoining houses have different front elevation plans. Front elevation plans must include architectural features which sufficiently differentiate them from the adjoining house plan.*
 - b. *On other lot situations only if the adjoining houses include reverse plans and have different front elevation plans. Front elevation plans must include architectural features which sufficiently differentiate them from the adjoining house plan.*
3. **CARRIAGE UNITS.** *Carriage units may be permitted by the decision-maker through the approval of a Planned Development in single family subdivisions. A carriage unit is a second dwelling on a lot that is located above a detached garage. Carriage units are subject to the following standards:*
 - a. *Minimum Lot Size. Carriage Units or Granny Flats may be located only on lots with 4,500 square feet or more of area.*
 - b. *Floor Area Ratio. The maximum floor area ratio for lots with Carriage Units or Granny Flats shall be .5.*
 - c. *Location of Carriage Unit. Carriage Units or Granny Flats are only permitted over detached garages on alleyways. No more than two contiguous lots shall contain Carriage Units.*

- d. *Number of Carriage Units.* No more than 50 percent of the lots in a subdivision shall contain Carriage Units.
 - e. *Off-Street Parking.* In addition to the required parking for the main dwelling unit, a third parking stall (covered or uncovered) shall be provided adjacent to the garage for the granny flat. The parking space shall be exclusive of the yard requirement for the lot.
4. **ALLEYS.** Alleys may be permitted by the decision-maker through the approval of a Planned Development (PD) in single family subdivisions. An alley is a public or private street primarily designed to serve as secondary access to the side or rear of those properties whose principal frontage is on another street. Alleys are subject to the following standards:
- a. *Where Permitted.* Alleys shall be permitted only through approval of PD for projects and locations where deemed appropriate. It is the City's policy that public alleys only be provided in unusual situations where other forms of access are not practical.
 - b. *Minimum Width of Alley.* The minimum width of an alley shall be 20 feet.
 - c. *Alley Access.* Garages with access from an alley should be detached. Garages may be attached to the main dwelling unit if they are connected by an open trellis or breezeway.
 - d. *Parking.* Parking shall not be permitted in an alley unless in parking bays.
 - e. *Setbacks.* Garages must be set back a minimum of five feet from an alley. Automatic garage door openers shall be provided for all units with garages less than 20 feet from an alley.
 - f. *Through Traffic.* Alleys shall be designed to discourage through traffic.
 - g. *Maintenance/Security.* Alleys and adjacent development shall be designed to promote security and attractive appearance. Project approval shall require features such as:
 - (1) Dwelling units designed to provide visual access to alley areas;
 - (2) Open style fencing along abutting rear yards;

- (3) *Security lighting;*
- (4) *Street addresses provided at intersection of alley and public street;*
- (5) *Public alleys shall be approved only when a maintenance district is concurrently established;*
- (6) *Maintenance of private alleys shall be addressed through CC&R provisions.*

5. PRIVATE ROADWAYS. *Private driveways serving more than 6 dwelling units may be permitted by the decision-maker through the approval of a planned development in single family subdivisions (see Section III.B.3. for standards applicable to common driveways serving 6 or fewer units). In general, private driveways are not appropriate in standard single family residential subdivisions.*

a. *Private roadways shall be constructed to meet all City street standards or to appear significantly different than public streets. Examples of design concepts for private roadways that look significantly different than public streets include:*

- (1) *Valley gutters;*
- (2) *Sidewalks on one side of the drive only;*
- (3) *Non-standard radius;*
- (4) *Landscape features.*

b. *Private roadways shall comply with the following standards:*

- (1) *Private roadways shall be designed to provide adequate fire and disposal service vehicle access;*
- (2) *Public water lines may be located within private drives with a public utility easement (PUE); the lines shall be ductile iron construction to minimize future maintenance; a hold harmless agreement shall be recorded to protect the City against claims when City maintenance is required;*
- (3) *Sewer, storm drains, and all other utilities must be private;*

- (4) *Paving and sub-base sections of the private drives must be constructed to City standards;*
- (5) *A homeowners association must be established with adequate assessments to pay for long term maintenance.*
- (6) *Courts shall not be greater than 150 feet in length. A fire hydrant shall be provided at the entrance to each court in accordance with Fire Department standards.*
- (7) *Improvement plans shall be submitted for review and approval by the City Engineer.*

6. PARKING STANDARDS FOR RESIDENTIAL DEVELOPMENTS WITH PRIVATE ROADWAYS. *A minimum of four parking spaces for each dwelling unit in the subdivision shall be provided as follows:*

- a. *No less than 2 parking spaces for each unit within a detached or attached garage;*
- b. *No less than .5 spaces for each unit in a parking bay or common parking area;*
- c. *The remainder of the required parking may be provided in driveways, parking bays, or common parking areas;*
- d. *A reduction to these standards may be granted by the decision-maker with sufficient justification provided by the applicant that the project will not require the standard number of parking spaces.*

7. COMMON DRIVEWAYS. *Common driveways will be permitted for flag lots and similar situations subject to the following standards.*

- a. *Construction standards for common driveways shall be a minimum of 5 inches of concrete unless a soils report recommends a greater section; width, curb and gutter type and other design features would be determined by the City Engineer.*
- b. *Maximum number of 6 dwelling units shall be served via a common driveway.*
- c. *Minimum width of paving shall be 20 feet; which shall be posted "No parking."*

- d. *Driveway and associated improvements shall be located within a private access easement; a mechanism for maintenance of improvements shall be required as part of the subdivision or parcel map.*
- e. *Associated improvements may be required, such as walkways, security lighting, and landscaping (5 feet of landscaping is typically required on both sides of common driveway).*
- f. *Addresses of all dwelling units served by a common driveway shall be posted on a sign located at its intersection with a public street.*
- g. *Maximum grade of driveway shall not exceed fifteen (15) percent.*
- h. *If the length of the driveway exceeds 150 feet, a turn-around for fire vehicles shall be provided.*
- i. *Additional on-site parking may be required if the decision-maker determines that on-street parking in the vicinity is inadequate.*
- j. *Fence height adjacent to the common driveway shall not exceed 3 feet in height when that portion of the lot corresponds to a front yard setback.*
- k. *No dwelling unit shall be located more than 150 feet from a fire hydrant.*
- l. *No public utilities or public landscaping shall be located within the common driveway; separate sewer and water lines shall be required for each lot.*

8. RECREATION/OPEN SPACE FOR PROJECTS WITH A MINIMUM LOT SIZE OF LESS THAN 4,500 SQ. FT.

- a. *Each unit shall be provided with a minimum private yard area, located between the rear of the house and the rear property line, of 400 square feet with a minimum dimension of 10 feet, exclusive of any parking area. The decision-maker may approve a reduction to this standard only if additional common recreation/open space is provided in excess of the requirements set forth below in Item b.*

- b. *In addition for projects with 30 or more dwelling units, a privately owned common open space area or areas with a minimum of 200 square feet per dwelling unit shall be provided on the project site. This common area shall contain recreational facilities as determined through approval of a PD. The privately owned common open space area may contain facilities such as pools, clubhouses, tot lots, meeting rooms, tennis and/or volleyball courts, etc. The intent of this provision is that the common recreation area would provide recreation opportunities in lieu of those that would be provided in a standard rear yard area.*

If the decision-maker finds that the provision of the common open space within the project is not feasible or undesirable, the decision-maker may approve one of the following alternatives:

- (1) *Alternative #1: Additional Private Open Space. Additional private open space may be approved in lieu of common open space if the following conditions are satisfied:*
 - (a) *Each dwelling unit shall have a private yard area located behind the dwelling unit with a minimum area of 900 square feet, and*
 - (b) *The average area of all rear yards within the project shall be at least 1000 square feet.*

- (2) *Alternative #2: Off-site Recreational Facilities. Off-site recreational facilities may be approved if the following conditions are satisfied:*
 - (a) *The project is located within reasonable proximity of an existing neighborhood or community park, with direct access via a public street or pedestrian access easement,*
 - (b) *The developer installs additional recreational improvements to the park, dedicates additional land to the park, or a combination thereof which have the same approximate land and improvement dollar value as providing common open space within the project,*
 - (c) *The additional recreational improvements are consistent with the City's Comprehensive Parks, Recreation, and*

Open Space Master Plan and any approved plans for the adjacent park, and

- (d) *The cost of the additional recreational improvements shall not be eligible for development impact fee credit or other reimbursement by the City.*

9. NUMBER OF HOUSE DESIGNS IN A SUBDIVISION. *All production single family residential developments shall include floor plans for homes proposed in the subdivision as follows:*

- a. *Ratio of House Designs to Lots. Every residential development (except custom home projects) shall include no less than one house design for each 30 lots. House designs shall be substantially different and contain architectural features that highlight such differences. Every house design shall also have no less than three alternative front elevation plans with substantially different architectural treatment. A subdivision with less than 60 units shall include at least three house plan designs.*
- b. *Maximum Number of Residences Utilizing the Same House Plan Designs. The maximum number of dwelling units constructed within a development which utilize the same house design shall be 200.*
- c. *Master Plotting Plan Required. A master plotting plan that indicates the house plan design or floor plan proposed for every lot shall be submitted with every production home development project. The plan shall include as a minimum: the model, including reverse plans; delineation of first and second story building footprints; garage locations; driveways; and building setbacks.*

10. NEIGHBORHOOD IDENTITY. *Large subdivisions shall be designed to accommodate different neighborhoods that have separate identities.*

Generally, these neighborhoods should contain no more than 200 residences. This type of neighborhood identification can be achieved by providing different street trees and landscape themes, product types, lot widths, lot sizes, and architectural styles, entry treatment, and park theme.

- a. *Subdivision Entry Features. Neighborhoods should include entry features to provide identity and recognition. For example, landscaped medians, increased landscape area and materials at corners, project identification signage (monument or subdivision wall signs only) may be used to achieve these objectives. When determining the design and materials for the entry feature, consideration should be given to*

maintenance and vandalism. These features shall be designed to provide adequate sight distance when located adjacent to streets or at street intersections.

- 11. MIX OF ONE AND TWO-STORY UNITS.** *At least twenty-five (25) percent of the homes within a residential development shall be one-story and at least twenty-five (25) percent shall be two-story. When only three house plan designs are provided in accordance with Section 8.a. above, then at least thirty (30) percent of the homes shall be one-story and at least thirty (30) percent shall be two-story. (Reference Section No. _____)*
- 12. MIX OF HOUSE PLAN DESIGNS.** *Each house plan design shall be constructed on a minimum of 15 percent of the total number of lots and a maximum of 40 percent of the lots. The percentage shall be based on the number of house plan designs and the total number of lots in the subdivision.*
- 13. PEDESTRIAN AND BICYCLE ACCESS.** *Streets, pedestrian paths and bike paths should contribute to a system of fully-connected and interesting routes to all destinations. Their design should encourage pedestrian and bicycle use by being small and spatially defined by buildings, trees and lighting; and by discouraging high speed traffic. Bike paths should be provided on streets and in adjoining creekway areas in accordance with the City's Parks and Recreation Master Plan.*
- 14. OPEN SPACE.** *Common open space areas are encouraged in the residential subdivision. Sites for neighborhood and community parks shall be incorporated into projects when designated in the Comprehensive General Plan and Parks and Recreation Master Plan. Significant natural features, such as wetlands, creekways, hillsides, habitat protection areas, should be preserved in residential subdivisions. Wherever possible, the natural terrain, drainage, and vegetation of the community should be preserved in these open space areas. Convenient and safe access from residences to these areas should be provided. The subdivision design should place streets adjacent to the open space for public safety and access. Homes should face rather than back toward open space areas. All open space areas should have good visibility for police patrol units.*
- 15. STANDARDS FOR MODEL HOMES AND COMPLEXES.** *Models, or residential units designed and improved to temporarily display and market homes in the subdivision, are permitted but not required for development projects. Model homes and complexes shall be constructed in accordance with the following standards:*

- a. *Design Review is Required for All Models.* A site plan indicating the lots on which the models are to be constructed shall be submitted with the application. The location and details of the sales office shall be shown on the plans. The site plan shall also provide the location of all proposed off-street parking, fencing, and all other on and off-site improvements associated with the models.
- b. *Parking.* No less than one on or off-street parking space shall be provided for each model. Parking spaces shall be located within 600 feet of the model complex and comply with all standards in the Land Use and Development Code.
- c. *Conversion of Office Space back to Living Area.* Sales offices, signs, and temporary improvements associated with the model complex shall be removed prior to occupancy of the last dwelling unit constructed in the subdivision. Sales offices located in an approved garage space shall be converted back into garage area.
- d. *Signs.* Signs for model complexes are permitted in compliance with Chapter 17.48 of Title 17 and as set forth above in A.8.c.(3).
- e. *Model complexes shall be reviewed and approved by the Police Department.* Model homes should include alarmed entries, security lighting, phones (inside), and panic alarms.

- 16. CONDITIONS, COVENANTS, AND RESTRICTIONS (CC&R'S) REQUIRED.** *CC&R's shall be prepared for all subdivisions and submitted to the Director for review and approval prior to the final map. The CC&R's shall include a provision that recreational vehicles may not be stored in the front yard of any dwelling unit and any other provision(s) required by the decision-maker in conjunction with the project.*

C. GUIDELINES

- 1. ONE AND TWO-STORY PLANS IN A ROW.** *There shall be no more than four one-story or two-story house plans in a row on the same side of the street, except in approved cul-de-sac situations.*
- 2. DUPLEXES ON CORNER LOTS.** *Duplex units are encouraged on corner lots to increase the diversity of housing and to provide more affordable housing opportunities for families. Entryways and garages for each dwelling unit should be oriented toward different street frontages.*

3. **THEME STREET LIGHTS.** *Use of decorative street lights in theme with the design of the subdivision is encouraged for private streets.*
4. **CURVILINEAR STREET PATTERN.** *Curvilinear streets are recommended in residential subdivisions to improve the streetscape appearance and reduce the visual impact of garage doors and two story structures. This type of street design also helps reduce vehicle speeds in residential neighborhoods. Grid street patterns may be permitted in special situations such as neighborhoods surrounding community or park facilities.*
5. **SIDE-ON CUL-DE-SACS.** *Side-on cul-de-sacs, that back onto arterial streets, are encouraged to enhance the appearance of the streetscape by providing variation in the location of homes visible from the adjoining street(s).*
6. **VARIATION OF LOT WIDTH.** *The width of lots should vary to provide diversity and visual interest in residential subdivisions. Cul-de-sac and curvilinear streets help achieve the objectives of this guideline.*
7. **RECYCLING AREAS.** *Subdivisions should include, when possible, the following features for the collection of recyclable materials:*
 - a. *Storage areas for recyclable materials collection containers should be provided on residential lots. Dimensions of the storage areas should accommodate containers consistent with current methods of collection in the area in which the project is to be located. These areas should be located behind the fence between the side property line and residence and constructed with concrete paving.*
 - b. *Driveways or travel aisles shall provide unobstructed access for collection vehicles and personnel and provide at least the minimum clearance required by the collection methods and vehicles utilized by the hauler in the area in which the development project exists.*
 - c. *Residential developers are encouraged to include recycling areas or systems within the residence; such as roll-out drawers below the sink for recycling receptacles; fire-rated, cleanable, secure chutes from the living space to the space for collecting and loading recyclable materials, etc. meeting Building Code requirements. Recommended internal storage space for individual living units of residential development project is three (3) cubic feet.*

8. **TRIP REDUCTION FEATURES.** *Whenever possible, subdivision design should incorporate features that encourage the reduction of vehicle trips. Such features may include designated and centralized areas for posting rideshare information (kiosks, etc.), bus shelters, bus turnouts, connections between neighborhoods and bike trails, convenient access to alternative transportation, etc.*

9. **ENERGY EFFICIENT DESIGN.** *Street orientation, the placement of residences, and use of shading should contribute to the energy efficiency of the community.*

III. SITE PLAN DESIGN (FOR INDIVIDUAL LOTS)

A. ORDINANCE REQUIREMENTS

1. **COMPLIANCE WITH SETBACK AND OTHER DEVELOPMENT STANDARDS.** *All dwelling units shall comply with the Zoning Ordinance, Policy Plan, or Specific Plan standards (whichever are applicable). This includes, but is not limited to, requirements for: minimum lot area; minimum site width and depth; minimum front, side and rear yards; minimum site frontage; minimum setbacks adjoining a freeway, arterial, railroad right-of-way; minimum distance between structures; maximum site coverage; maximum building height; and alternative housing configuration requirements. The Residential Districts Development Standards Matrix, attached as Table 1, provides a summary of these residential standards.*

If the project does not include normal yard areas, setbacks for pools, patio covers, and accessory structures shall be established in the Planned Development. The Conditions, Covenants, and Restrictions for the subdivision shall include the setbacks established with the approval of the Planned Development or Design Review.

(Reference Section No. _____)

2. OFF-STREET PARKING. *Every residential project shall include both off-street and street parking. Parking requirements for subdivisions on private streets are set forth in Section II.B.5., above. All other residential projects shall be provided with parking as follows:*

- a. *Garages/on-site parking. Each dwelling unit shall include two parking spaces within an enclosed attached or detached garage. The garage design shall be architecturally integrated with the dwelling. The minimum inside dimension of the each garage space, exclusive of water heaters, furnaces, washer and dryer, or other equipment, shall be 9' 2" wide by 20' feet deep. One step 9" or less or similar projections that do not interfere with the vehicle parking may encroach into the garage space.*

Tandem garage spaces (a garage space in front of or behind another garage space) may be permitted on narrow lots less than 60 feet wide when the decision-maker finds that the appearance of the dwelling will be improved with the proposed design. Tandem spaces are permitted on lots 60 feet and wider when provided in addition to a standard two-car wide garage. The minimum interior dimensions of a tandem parking space are 10 feet wide by 20 feet deep.

- b. *Street parking. See Street Parking in Subdivision Design, Section II.A.1.
(Reference Section No. _____)*

3. FRONT SETBACK VARIATION. *Front yard setbacks should be varied to increase visual interest and reduce the canyon effect in subdivisions. Garage, living area, and porch setbacks in the front yard should vary to provide a more interesting streetscape appearance. Garage front yard setbacks shall vary from a minimum of 18 feet to 22 feet or more. This effect may also be achieved by recessing the garage to allow the living area to extend closer to the front property line while still maintaining the required yard area. (Reference Table No. 14.074.01, footnote 2.b.i.)*

- (1) *The front yard and street side yard area of a corner lot shall be cross ripped to a depth of 12 inches and rototilled to a depth of 6 inches prior to final inspection to break up soil compaction;*
- (2) *The initial domestic water connection stub for connecting the front yard irrigation shall be installed prior to final inspection;*
- (3) *The amount of turf installed, if any, shall be in accordance with the requirements of the Water Efficient Landscape Regulations;*
- (4) *Conditions, covenants, and restrictions (CC&R's) for new subdivisions shall include a provision that front yard, and if applicable the street side yard area of a corner lot, be landscaped within one year of occupancy of the dwelling.*

(Reference Section No. 14.074.130)

5. **MAXIMUM PAVED AREA.** *No more than fifty (50) percent of the front yard shall be paved with concrete, asphalt, or other similar impervious materials. (Reference Section No. 14.074.130)*
6. **PERMANENT FOUNDATION REQUIRED.** *All permanent dwelling units shall be constructed on or attached to a foundation, except for mobile homes located in an approved mobile home park. (Reference Section No. 14.074.110)*
7. **LOT COVERAGE.** *The maximum percentage of lot covered by building area, exclusive of eave overhang, shall be 40 percent. The percent of the site area covered by structures shall be measured by dividing the number of square feet of horizontal area covered by structures, open or enclosed, by the total horizontal area within the property lines of the site. (Reference Section No. _____)*
8. **SETBACK EXCEPTIONS – MAXIMUM ENCROACHMENT OF ARCHITECTURAL FEATURES.** *Architectural features, including but not limited to sills, chimneys, cornices, and eaves, may extend into a required side yard or a space between structures not more than two feet and may extend into a required front or rear yard not more than six feet. These architectural features do not include posts, walls, bay windows that extend to the ground, media niches, or other similar designs. (Reference Section No. _____)*

- 9. HOME SAFETY AND SECURITY.** *All residential units shall be constructed in accordance with safety and security requirements set forth in Chapter 14.282 of Title 14. All dwelling units shall display a street or building number in a prominent location such that the number is easily visible to approaching emergency vehicles. Dwellings with alley access should include address numbers that are visible from both the street and alley. The numerals shall be no less than 3 inches in height and shall be of a contrasting color to the color of the background to which they are attached. All address numbers shall be internally-illuminated continuously during the hours of darkness.*
(Reference Section No. 14.282.120)
- 10. DRIVEWAYS.** *Driveways shall conform to the following standards:*
- a. *No person, firm, or corporation shall construct or maintain any driveway across any curbing or sidewalk, or connection with any uncurbed roadway, without first securing a city permit. All construction of such driveways shall be in conformity with city standards and specifications, and shall be subject to city inspection.*
 - b. *Driveways for residences serving not more than two garage spaces shall have a minimum width of 12 feet.*
 - c. *No driveway shall be located within an intersection crosswalk, which is defined by the prolongation of the boundary lines of sidewalks.*
 - d. *No driveway shall be less than five feet from the center of a traffic signal pole, fire hydrant, utility pole, or street light standard.*
(Reference Section No. 17.36.060)
- 11. FENCING.** *Fencing, both subdivision perimeter and interior, should be designed as an integral element of the subdivision (See Section II.A.4. for perimeter fence/wall requirements). A fencing plan should be incorporated into the development plan for the project. Fencing materials should be durable and aesthetic. Individual lot fencing, when installed, should meet the following standards:*
(Reference Section No. 14.074.120)
- a. *Acceptable fencing materials include masonry, brick, wrought iron, chain link, or other tubular metal materials, and wooden fencing, excluding plywood or other composite panels or materials.*

- b. *The use of retaining walls should be minimized. Sloping, gravity-type walls are preferred when retaining walls are necessary due to grade differences. Retaining walls exceeding one (1) foot in height shall be masonry.*
- c. *Corner lot fencing shall be placed not closer than fifteen (15) feet from the face of curb on the street side of a corner lot. All fences shall be returned to the adjacent dwelling per the requirements of the Director. All required fencing shall be shown on plot plans submitted at the time of building permit application and shall be located and constructed in a manner consistent with the details and specifications approved by the Director. Such fencing shall be installed prior to occupancy of individual homes.*
- d. *All fencing shall comply with the height requirements of the particular Zoning District and/or Policy Plan/Specific Plan in which it will be located. In general, fencing should not obstruct views of motorists either driving on streets or backing out of a driveway.*

B. STANDARDS

- 1. **DRIVEWAYS.** *Driveways should be designed consistent with the front yard landscaping. Use of brick, bominite, aggregate and salt finish, coloring, and patterns in driveways is recommended. Where the driveway provides access to three or four-car garages, the driveway should be reduced in width (tapered) at the street to reduce the hardscaping. Driveways shall conform to the following standards:*
 - a. *Not more than fifty percent of the frontage upon any street of the parcel of land to be served thereby shall be devoted to driveways, except that in cases of narrow frontages on cul-de-sacs, normal driveways may be permitted subject to approval of the City Engineer.*
 - b. *Driveways on the same property shall be at least twenty-four feet apart.*
 - c. *Driveways located in proximity to street intersections should in general satisfy the following minimum criteria and shall be otherwise located if so required by the City Engineer: residential driveways shall not be located closer than forty feet from the curb return unless a different distance is approved by the City Traffic Engineer; no driveway shall be located within the curved portion of a constant radius curb return in*

any case except for curb returns of greater than sixty feet constant radius where driveway encroachment may not exceed one-eighth of the total arc length.

- d. *Driveways may start from property line, but in no case may driveways on adjoining properties be closer than five feet except when mutual easements described in deeds provide for a common driveway along the property line. Such common driveways shall be subject to all other requirements and specifications.*
- e. *Adjustments to utility facilities or other public improvements shall be accomplished without cost to the city.*
- f. *Any abandoned driveway shall be reconstructed to standard city sidewalk, curb, and gutter requirements, concurrent with the new driveway construction without cost to the city.*

2. FLOOR AREA RATIO (BUILDING INTENSITY). *Floor area ratio of the house, excluding the garage, generally should not exceed 0.45; house size would be determined through the design review or PD approval and would take into consideration the architectural design of units, lot width, project density and type of housing in neighborhood.*

3. STREET TREES. *All dwelling units/lots shall include a street tree (minimum 15 gallon 1 1/2 inch trunk caliper measured four feet above finished grade. Street trees should be used to emphasize the neighborhood design theme, provide shade, and create improved streetscape views.*

4. MINIMUM EAVE OVERHANG. *The minimum roof extension or overhang beyond the building walls of the dwelling shall be 12 inches. The minimum overhang may be reduced by the decision-maker based upon the architectural style of the dwelling.*

5. FRONT YARD AND CORNER LOT STREET SIDE YARD LANDSCAPING FOR RLM 3.6 AND RLM (C) LOTS. *Special consideration shall be provided for the front yard and corner lot street side yard landscaping of RLM 3.6 and RLM (C) lots. The location of utility and sight line corridors shall be coordinated with the placement and design of all landscape materials and irrigation devices. A conceptual landscape plan shall be submitted with the PD or design review application. Landscaping shall be installed prior to final inspection of the dwelling, and shall comply with the Vacaville Water Efficient Landscape Regulations and Chapter 14.074 of the Land Use and Development Code.*

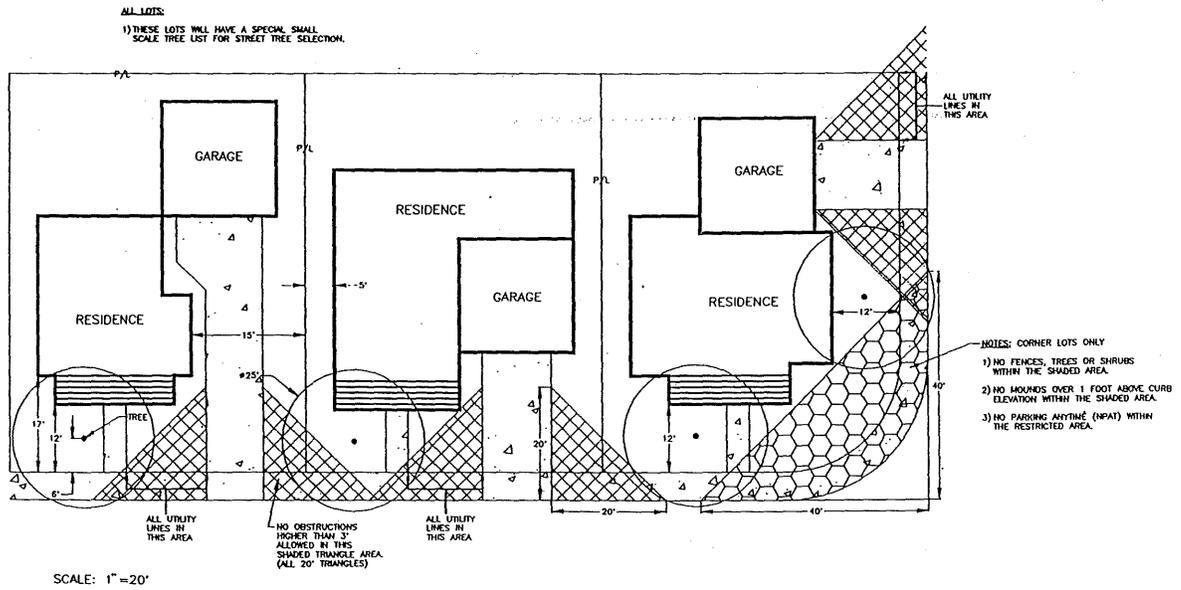


Figure ____. Illustration of front and street side yard landscaping considerations on RLM 3.6 and RLM (C) lots.

IV. ARCHITECTURAL DESIGN

A. ORDINANCE REQUIREMENTS

1. **DESIGN REVIEW APPROVAL REQUIRED.** *All single family homes are subject to review with the consideration of a design review or planned development application. (Reference Section Nos. 14.111.040 and 14.113.040)*
2. **SCREENING OF MECHANICAL EQUIPMENT.** *Mechanical equipment on residential lots shall be screened in accordance with Chapter 14.074 of the Land Use and Development Code. Satellite dishes with a diameter greater than one meter shall comply with the provisions of Chapter 17.63, Performance Standards, of the Zoning Ordinance. (Reference Section No. 14.074.150)*

B. STANDARDS

1. **STYLE OF ARCHITECTURE – VARIED AND DISTINCTIVE.** *Architectural design of the residences in a model complex or neighborhood shall be varied and distinctive and may include styles such as Monterey/Spanish, English/French Country, Craftsmen, Prairie/Contemporary, Mediterranean, etc. Architecture should also be compatible with surrounding residential neighborhoods. House elevations should be varied and include irregularly shaped windows, gabled vents, shutters and/or plant shelves, or other distinctive architectural features.*

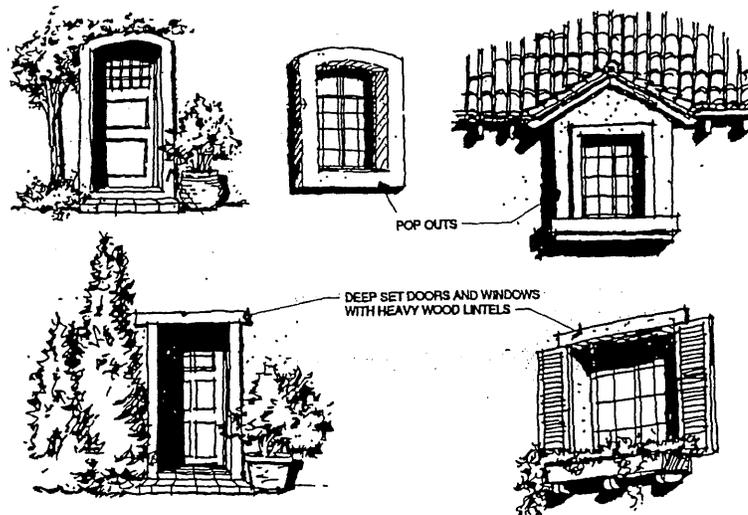


Figure 4. Examples of trim elements.

2. **WALL PLANES.** Building offsets, roof overhangs, bay windows and other features should be used to provide visually interesting houses. Large, unbroken wall planes are discouraged on any elevations. Elevations facing arterial streets or open space should include enhanced features and building materials.
3. **SIDING MATERIALS.** Acceptable siding materials include, but are not limited to, architectural grade grooved plywood siding, masonite siding, lap siding, horizontal siding, vertical siding, diagonal siding, shingles, and stucco. T-1-11 (5/8" or better; 3/8" is not permitted) or equivalent grooved plywood siding shall be restricted to side and rear elevations. Plain plywood siding shall be prohibited, except where used for accent purposes.

Brick, stone, or other masonry element shall be incorporated in at least one of the three alternative front elevations for each of the models. The masonry element shall be used in sufficient quantity to be a consequential feature of the facade. For example, the masonry element should be extended the full height of columns or to the base of windows and other openings when possible. A significant architectural feature may be used in place of a masonry element with the approval of the Community Development Director.

4. **MATERIALS AND FEATURES REQUIRED ON SIDE AND REAR ELEVATIONS.** *Front elevation design features and materials shall be returned a minimum of four feet on side elevations, except that materials simulating columns or similar features may be returned the equivalent width of the feature approved on the front elevation. On street side elevations, architectural treatment shall be used that is equivalent to the front elevation building materials and features. Street facing rear elevations shall include trim around windows, vents, and door openings, decorative windows, and other architectural enhancements that improve the appearance of the residence.*

5. **EXTERIOR COLORS.** *Typically, the exterior of new homes should be painted earth tones or pastels, which include a wide range of colors. Contrasting or bright colors should only be used for accent or trim. At least six (6) color combinations (base and trim) shall be provided for each model complex. The color scheme shall be submitted for review and approval by the Director.*

6. **ROOFING.** *Roofs are a major element of the house that should provide visual variety and interest. Roofs should include several planes at different heights. Different types of roofs, such as gable, shed, and hip, should be incorporated into the models. Roofing should complement the architectural features and materials on the remainder of the residence.*

Materials shall be tile, treated wood shake (except in hillside areas), or architectural grade (minimum 40-year) composition shingle. At least two color combinations shall be provided for composition roofing. Roofs shall have a minimum 5:12 slope. Architectural styles with a parapet (Bauhaus contemporaries, Santa Fe adobes, etc.) or low pitch roof (Craftsman bungalows, etc.) may be permitted if other significant features are provided in the design of the dwelling.

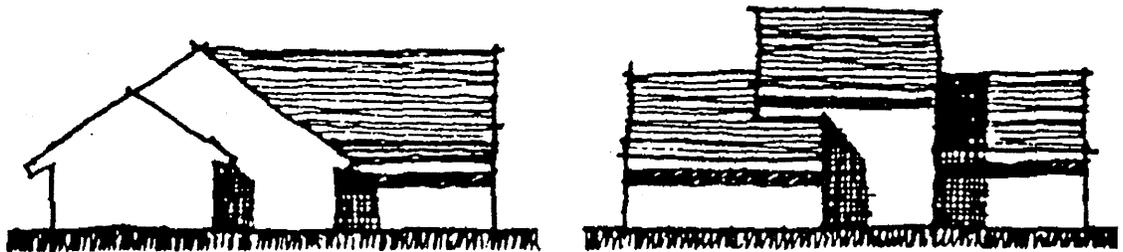


Figure 5. Recommended change in roof line.

7. **WINDOWS/VENTS.** *Residential dwelling unit design should include consideration of window design, detailing, and placement. Windows and vents on all elevations, especially street side and rear, should include trim elements, window mullions, or other details. Front windows should include heavier trim, masonry or wood accents, pop-out features, pot shelves, or roof elements over the windows. Use of bay and other decorative type windows is recommended.*

8. **WINDOW/PRIVATE OPEN SPACE ORIENTATION.** *Living area, such as bedrooms, should be oriented so that the windows do not directly face dwelling unit living areas on adjacent lots. Private yard areas should also be situated on lots to provide reasonable privacy. Window and private open space orientation is especially important for higher density projects and developments with less than standard setbacks.*

9. **GARAGE DESIGN.** *Garages should be designed so they do not dominate the front house elevation; the design should also reduce the mass and uninterrupted wall planes of the garage. Garages should generally be no more than 50 percent of the width of the house. The minimum width of the house, excluding the visible garage wall, shall be not less than 10 feet or 30 percent of the total width of the residence (including garage), whichever is greater. Side entry garages, detached rear entry garages, and garages that are setback from the main house should be used whenever possible. In addition, decorative windows, offsets, columns, and other features should be incorporated into garages. The second story above the garage should be setback from the plane of the garage wall and should include decorative windows, special roof features, or other features to reduce visual impacts.*

Detached garages, located behind the residence, are encouraged on narrow lots to improve the street appearance of the house. Detached garages may include access from the street along the side of the residence or from an alley or street adjoining the rear property line.

Three-car garages or garages wider than twenty-two (22) feet shall only be permitted on lots with 60 feet or more of width, measured at right angles to the depth at a point midway between the front and rear property lines. Bonus rooms or other attached or unfinished space adjacent to the garage and visible from the street shall include front-facing quality building materials and architectural features. Offsets, windows, columns, and a change in roof design and/or plane should be used for these wider garages. Tandem garages are encouraged to provide necessary third and fourth garage spaces.

All residential units shall have garages with sectional, roll-up garage doors.

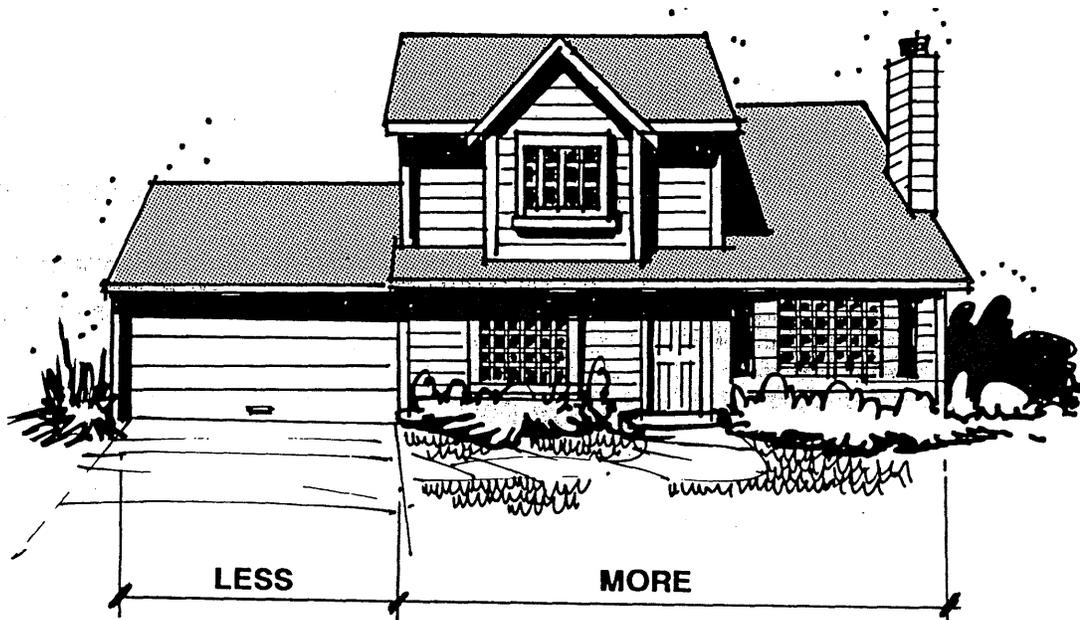


Figure 6. Recommended decrease in width of garage doors and increase living area facing street.

- 10. ENTRY TREATMENT.** *Residences should be designed to provide visible front entrances for public safety and to promote a sense of community. As additional incentive, front setbacks for unenclosed porches are approximately five feet less than for the main structure as set forth in the attached Table 1.*

A front entry should be designed as a functional and architecturally integral extension of the interior living area. Examples of front entry treatment include, but are not limited to inset entries, portico elements, porch elements, etc. Raised front porches shall be completely finished to match the house design; skirting constructed of material to match the house siding shall be provided for raised porches to conceal all structural elements.

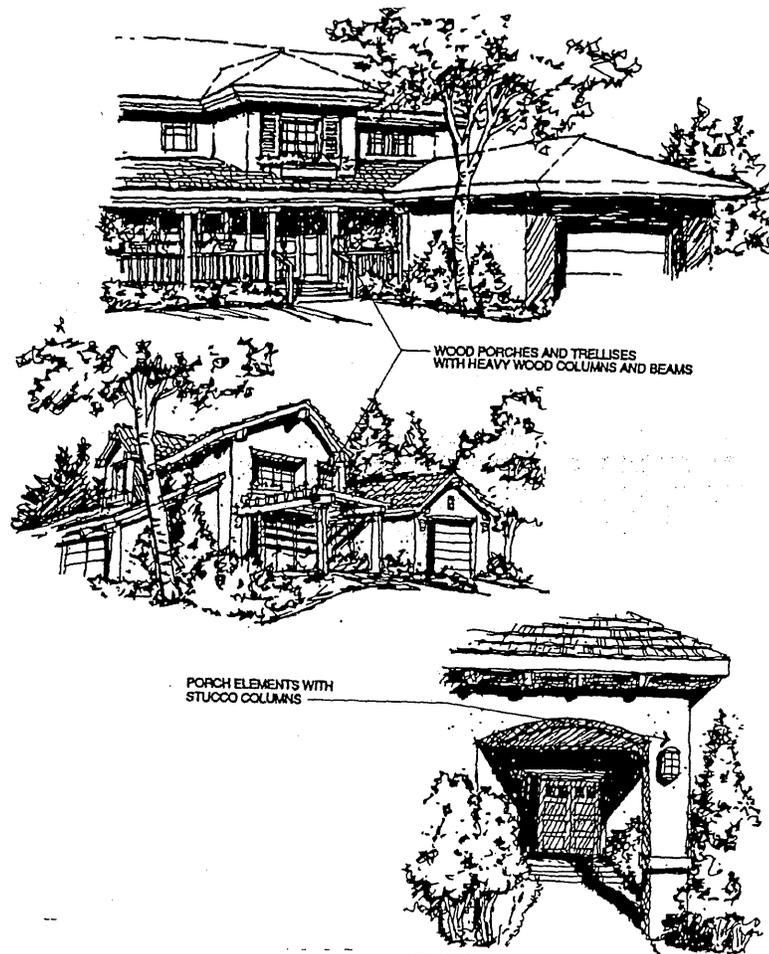


Figure 7. Recommended Entry Treatment

11. SECURITY DESIGN. *Residences should be designed to provide a safe and secure living environment for families. Design considerations should include:*

- a. *Landscaping height should be reduced around the front dwelling unit entry to provide adequate surveillance for police.*
- b. *Dwelling unit entries should be located and designed to be fully visible from an adjoining street or alley.*
- c. *Glazing should not be located near the front entry doors.*
- d. *Living areas, such as bedrooms, should be oriented to face the front of the residence.*

APPENDIX 1

Min. Side Yard (ft.) (See Note 4)	Min. Side Yard - Street Side (ft.) (See Note 5)	Min. Rear Yard (ft.) (See Note 6)	Min. Site Frontage on a Public Street (ft.) (See Note 7)	Min. Setback Adjoining a Freeway, Arterial, or Railroad ROW (ft) (See Note 8)	Min. Distance Between Structures (ft.) (See Note 9)	Max. Site Coverage (%)	Max. Building Height (ft.)	Alternate Housing Configuration Requirement (See Note 10)
Perm./Cond.	Perm./Cond.	Perm./Cond.	Perm./Cond.	Perm./Cond.	Perm./Cond.	Perm./Cond.	Perm./Cond.	
30 / 30	30 / 30	40 / 40	100 / 100	40 / 40	10 / 10	5 / 5	40 / 40	n/a
25 / 25	30 / 30	40 / 40	100 / 100	40 / 40	10 / 10	5 / 5	40 / 40	n/a
20 / 20	30 / 30	40 / 40	100 / 100	40 / 40	10 / 10	10 / 10	40 / 40	n/a
20 / 20	30 / 30	40 / 40	100 / 100	40 / 40	10 / 10	10 / 25	40 / 40	n/a
20 / 20	25 / 25	40 / 40	100 / 100	40 / 40	10 / 10	25 / 25	40 / 40	n/a
20 / 30	20 / 25	20 / 20	40 / 40	40 / 40	10 / 10	35 / 25	40 / 40	n/a
15 / 25	20 / 25	20 / 20	40 / 40	40 / 40	10 / 10	40 / 25	40 / 40	n/a
10 & 15 / 20	15 / 20	20 / 20	40 / 40	40 / 40	10 / 10	40 / 25	40 / 40	n/a
10 / 20	15 / 20	20 / 20	40 / 40	40 / 40	10 / 10	40 / 25	30 / 30	n/a
10 / 20	15 / 20	20 / 20	40 / 40	40 / 40	10 / 10	40 / 25	30 / 30	n/a
10 & 5 / 20	15 / 20	20 / 20	40 / 40	40 / 40	10 / 10	40 / 25	30 / 30	n/a
10 & 5 / 20	15 / 20	20 / 20	40 / 40	40 / 40	10 / 10	40 / 25	30 / 30	n/a
10 & 5 / 20	15 / 20	20 / 20	40 / 40	40 / 40	10 / 10	40 / 25	30 / 30	25 %
10 & 5 / 20 or 5 & 5 / 20 or 0 & 12 / 20	10 / 20	20 / 20	40 / 40	40 / 40	10 / 10	40 / 25	30 / 30	50 %
5 & 5 / 20 or 0 & 10 / 20	10 / 20	15 / 20	40 / 40	40 / 40	10 / 10	40 / 25	30 / 30	100 %
15 / 20	15 / 20	15 / 20	40 / 40	40 / 40	10 / 10	40 / 25	30 / 30	100 %
20	20	20 / 20	40 / 40	40	10 / 10	40 / 30	40	n/a
20	20	20 / 20	40 / 40	40	10 / 10	40 / 30	40	n/a
20	20	40	40	40	10	30	40	n/a

TABLE 14.074.01

MINIMUM RESIDENTIAL DISTRICT DEVELOPMENT STANDARDS

(These standards apply to permitted and conditional uses in residential districts.
These are minimum standards. Greater standards may be required as a condition of project approval.)

Zoning District (See Note 1)	Density	Standards Indicator	Min. Lot Area (acres or sq. ft.) (See Note 2)	Min. Site Width (ft.)	Min. Site Width Corner Lots (ft.)	Min. Site Depth (ft.)	Min. Front Yard (ft.) (See Note 3)	Min. Front Setback to Unenclosed Porch (ft.)
			Permitted / Conditional Use	Perm./Cond.	Perm./Cond.	Perm./Cond.	Perm./Cond.	
RR	.1 to .4	10 ac	10 acres / 10 acres	400 / 400	400 / 400	200 / 200	50 / 50	45
		5.0 ac.	5 acres / 5 acres	250 / 250	250 / 250	200 / 200	50 / 50	45
		2.5 ac	2.5 acres / 2.5 acres	200 / 200	200 / 200	200 / 200	50 / 50	45
RE	.5 to 3.0	2 ac.	2 acres / 2 acres	175 / 175	175 / 175	200 / 200	50 / 50	45
		1 ac.	1 acre / 1 acre	150 / 150	150 / 150	200 / 200	50 / 50	45
		20	20,000 / 1 acre	100 / 150	110 / 150	100 / 100	30 / 30	25
		15	15,000 / 30,000	80 / 130	100 / 130	100 / 100	30 / 30	25
		12	12,000 / 24,000	80 / 100	90 / 100	100 / 100	25 / 25	20
		10	10,000 / 20,000	80 / 100	90 / 100	100 / 100	25 / 25	20
RL	3.1-5.0	10	10,000 / 20,000	80 / 100	90 / 100	100 / 100	25 / 25	20
		8	8,000 / 16,000	70 / 100	80 / 100	100 / 100	20 / 20	15
		6	6,000 / 12,000	60 / 100	70 / 100	95 / 100	20 / 20	15
		5	5,000 / 12,000	50 / 100	60 / 100	90 / 100	20 / 20	15
RLM	5.1 - 8.0	4.5	4,500 / 12,000	45 / 100	55 / 100	70 / 100	20 / 20	15
		3.6	3,600 / 12,000	40 / 100	45 / 100	60 / 100	15 / 20	10
		C (clustered)	10 acres / 12,000 (project area)	400 / 400	400 / 400	400 / 400	15 / 20	10 (to public street)
RM	8.1-14.0	n/a	7.5 acres / 12,000 (project area)	300 / 300	300 / 300	100 / 100	20 / 20	15
RH	14.1-20.0	n/a	5 acres / 12,000 (project area)	250 / 200	250 / 200	250 / 250	20 / 20	n/a
MHP	6.0-10.0	n/a	10 acres (project area)	400	400	400	20	n/a

RR: Rural Residential
RE: Residential Estates
RL: Residential Low Density
RLM: Residential Low Medium Density

RLM-C: Residential Low Medium Density-Clustered Units
RM: Residential Medium Density
RH: Residential High Density
MHP: Manufactured Housing Park, Including Mobile Homes

FOOTNOTES TO TABLE 14.074.01

- (1) **Zoning District:** For the RM, RH, MHP, and certain projects in the RLM-C district, the standards refer to the perimeter of the project site.
- (2) **Lot Area:**
- a. For the RLM-C, RM, & RH districts, the minimum area requirement refers to the total development area of a new project site.
 - b. Lots which were in existence prior to the effective date of this ordinance are developable subject to the requirements of this chapter.
- (3) **Front Yard:**
- a. **All Districts:**
 - i. Infill standards may apply; Section 14.074.080 should be reviewed for standards applicable to infill projects.
 - ii. For projects on private streets, setbacks may be established as part of the planned development process.
 - iii. Overhanging architectural features such as sills, cornices, and eaves without posts may extend into the required front yard up to 6 ft. This applies to the required front yard and does not apply to the front setback for porches.
 - b. **RR, RE, RL, RLM Districts:**
 - i. To encourage variation in front yard setbacks, a front yard setback between 18 ft. and 22 ft. may be allowed in the RL-8, 6, 5, and 4.5 districts when the average front yard setback on the street is 20 ft.;
 - ii. In the RLM-3.6 district, the front setback of 15 ft. may be applied only to the house, and not to the garage. The minimum setback to the garage shall be 18 ft.
 - iii. For RLM-4.5 and RLM-3.6: these are minimum standards for detached structures on public streets; other project standards may be altered through a planned development if fronting on a private street.
 - c. **RLM-C, RM, & RH Districts - Permitted Uses:**
 - i. For RLM-C, the 15 ft. setback applies to attached single family projects. Perimeter standards apply for other attached projects.
 - ii. On a site situated between sites improved with existing multi-family development, the minimum front yard shall be the average depth of the front yards of the adjoining sites, however, a depth of 10 ft. greater than the minimum front yard shall not be required;
 - iii. Where a site is situated between sites which are not improved with buildings and where sites comprising 40% of the frontage on a block are improved with buildings, the minimum front yard shall be the average of the existing front yards on the block, however, a depth of 10 ft. greater than the minimum shall not be required;
 - iv. To promote variation in front yard setbacks, when a multi-family development includes two or more adjoining structures, the decision-maker may require variation in front yard depths, provided that the average front yard depth shall be not less than the minimum depth required.
- (4) **Side Yards:**
- a. **All Districts:**
 - i. Infill standards may apply; Section 14.074.080 should be reviewed for standards applicable to infill projects.
 - ii. For projects on private streets, setbacks may be established as part of the planned development process.
 - iii. Architectural features such as sills, cornices, and eaves may extend into the required side yard or a space between structures up to 2 ft. Chimneys may extend into the required side yard or a space between structures up to 2.5 ft.
 - b. **RR, RE, RL, RLM Districts:**
 - i. In the RLM-4.5 and RLM-3.6 districts, the 0 & 12 ft. and 0 & 10 ft. setbacks refer to zero lot lines. Two zero lot line setbacks may not abut each other.
 - ii. Where a two-story portion of a structure or a portion of a structure exceeding 15 ft. in height is located closer than 15 ft. to a side property line, the minimum building setback shall be no less than 10 ft.
 - iii. In the RLM-3.6 district, a side yard abutting any two story portion of the structure may be 7 ft.
 - iv. For subdivisions approved prior to enactment of this ordinance, an existing dwelling may be extended along the existing legally established setback line.
 - v. In the RLM-4.5 district, for dwellings with 5 & 5 ft. setbacks, the following special standards apply: dwellings must be single story, and have a recessed garage located a minimum of 5 feet behind the front line of the habitable portion of the unit which contains the front entry to the dwelling and a covered porch.
 - vi. In the RLM district, garages located in the rear portion of the yard may be zero lot line.
 - c. **RLM-C, RM, & RH Districts - Permitted Uses:**
 - i. For existing lots of record established prior to enactment of this ordinance with a total area of 1 acre or less, the minimum side yard shall be 5 ft., subject to the following adjustments:
 - a. An additional 5 ft. shall be added for each story above the first story.
 - b. A side yard providing access to more than 1 dwelling unit shall be a minimum of 15 ft.

(5) Side Yard/Street Side:

- a. All Districts:
 - i. Infill standards may apply; Section 14.074.080 should be reviewed for standards applicable to infill projects.
 - ii. For projects on private streets, setbacks may be established as part of the planned development process.
- b. RLM-C, RM, & RH Districts - Permitted Uses:
 - i. For existing lots of record established prior to enactment of this ordinance with a total area of 1 acre or less, the minimum side yard shall be 10 ft.

(6) Rear Yard:

- a. All Districts:
 - i. Infill standards may apply; Section 14.074.080 should be reviewed for standards applicable to infill projects.
- b. RR, RE, RL, RLM Districts:
 - i. On a reversed corner lot, the minimum rear yard may be not less than the minimum side yard for the district, provided that the side yard adjoining the street shall be not less than the required front yard of the adjoining key lot.
 - ii. In the RLM-4.5 district, the required rear yard may be reduced to 15 ft. if an average setback of 20 ft. is provided.
 - iii. Architectural features such as sills, chimneys, cornices, and eaves may extend into the required rear yard up to 6 ft.
 - iv. In the RLM District, garages located in the rear portion of the yard may be zero lot line.
 - v. If the garage is located in the rear yard as an alternative configuration design, the rear yard setback and coverage standards may be adjusted through a planned development.
 - vi. Attached, unenclosed patio covers and solarium rooms may be located in the required rear yard provided that they shall be at least 10 ft. from the rear property line, and that the area of the structure in the required rear yard shall be included as part of the rear yard coverage regulations. Enclosed solarium rooms shall be entirely enclosed in clear material such as glass or plexiglass except for one side attached to the main structure.
- c. RLM-C, RM, & RH Districts - Permitted Uses:
 - i. For existing lots of record established prior to the enactment of this ordinance with a total area of 1 acre or less, the following shall apply:
 - a. The rear yard may be reduced to not less than 15 ft. provided one side yard is not less than 25 ft.
 - b. One foot shall be added to the rear yard for each 2 ft. of ht above the lowest 15 ft.
 - c. On a reversed corner lot, the minimum rear yard may be not less than the minimum side yard for the district, provided that the side yard adjoining the street shall be not less than the required front yard of the adjoining key lot.

(7) Site Frontage on a Public Street:

- a. More than one connection to the public street may be required.

(8) Setback for Sites Adjoining a Freeway, Arterial, or Railroad Right-of-Way:

- a. Infill standards may apply; Section 14.074.080 should be reviewed for standards applicable to infill projects.
- b. Setback shall be 40 ft. from property line or 50 ft. from face of curb on an arterial street.
- c. Alternate standards apply for accessory structures. See Section 14.074.140 of this chapter.

(9) Distance Between Structures:

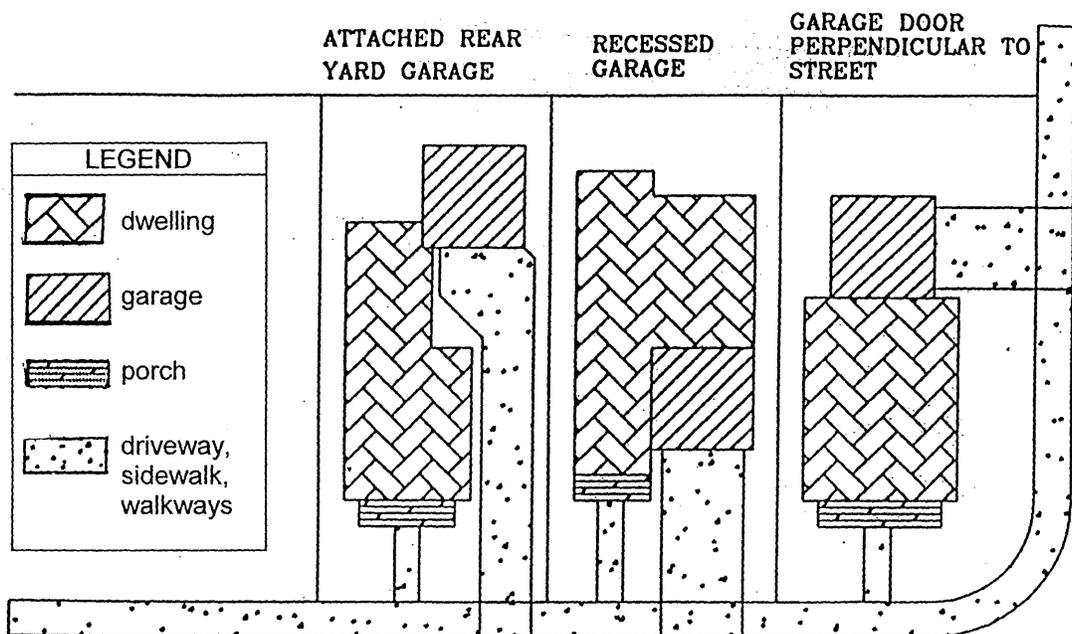
- a. Infill standards may apply; Section 14.074.080 should be reviewed for standards applicable to infill projects.
- b. The minimum distance for structures means either the distance between 2 structures meant for human habitation, or the distance between a structure meant for human habitation and another structure.

(10) Alternate housing configuration requirement in accordance with Section 14.074.090.

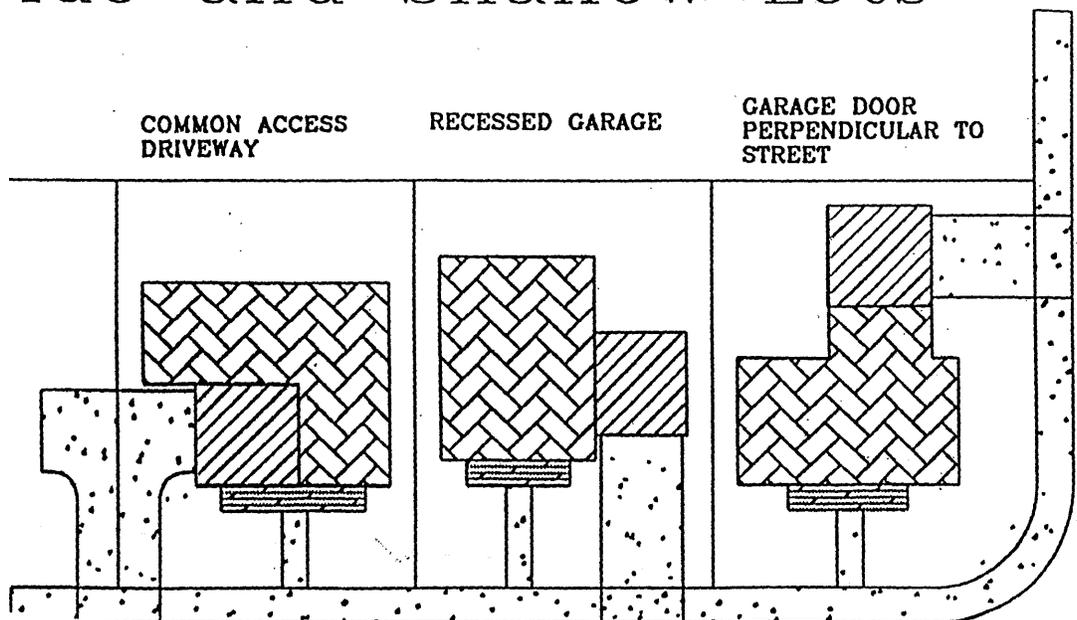
APPENDIX 2

EXAMPLES OF ALTERNATIVE HOUSE CONFIGURATIONS (WITH PORCHES) RLM 4.5 DISTRICT MINIMUM WIDTH LOTS

DRAFT - INTENDED TO BE ILLUSTRATIVE ONLY
(not to be adopted as part of this document)



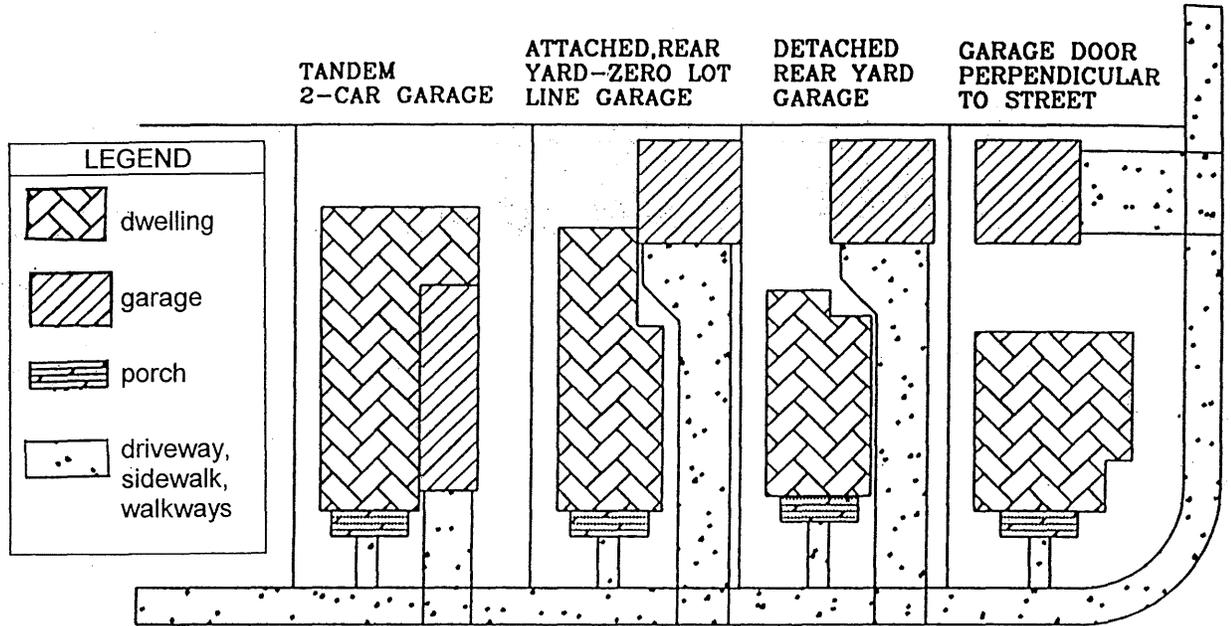
Wide and Shallow Lots



EXAMPLES OF ALTERNATIVE HOUSE CONFIGURATIONS (WITH PORCHES)

RLM 3.6 DISTRICT MINIMUM WIDTH LOTS

DRAFT - INTENDED TO BE ILLUSTRATIVE ONLY
(not to be adopted as part of this document)



Wide and Shallow Lots

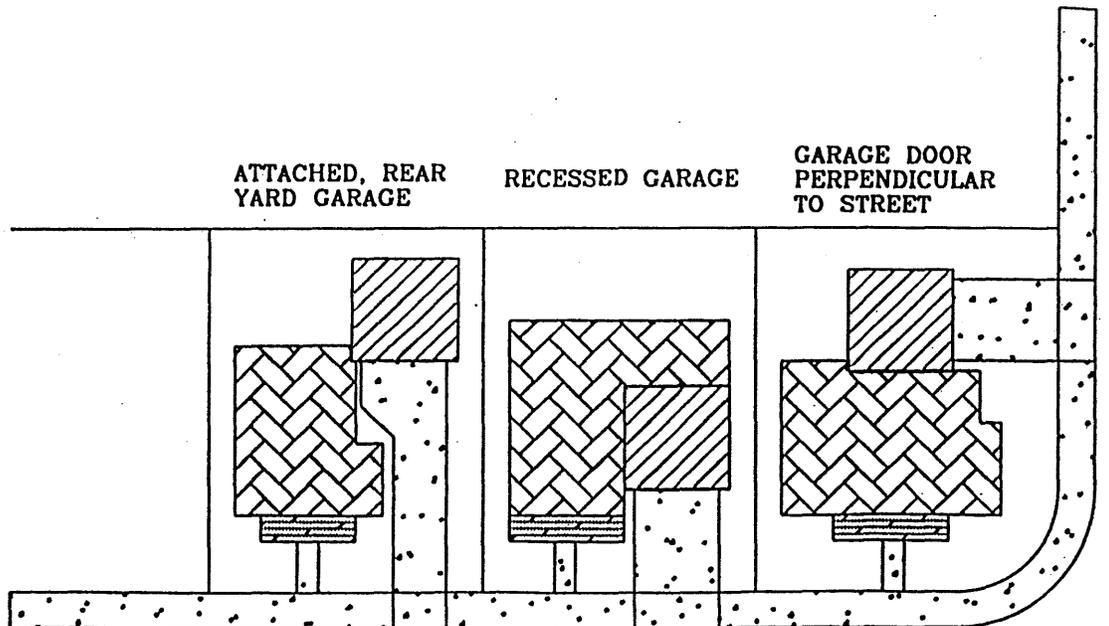


TABLE 14.074.01

MINIMUM RESIDENTIAL DISTRICT DEVELOPMENT STANDARDS

(These standards apply to permitted and conditional uses in residential districts.)

These are minimum standards. Greater standards may be required as a condition of project approval.)

Zoning District	Density	Standards Indicator	Min. Lot Area (acres or sq. ft.) (See Note 1)	Min. Site Width (ft.)	Min. Site Width Corner Lots (ft.)	Min. Site Depth (ft.)	Min. Front Yard (ft.) (See Note 2)	Min. Front Setback to Unenclosed Porch (ft.)
			Permitted / Conditional Use	Perm. / Cond.	Perm. / Cond.	Perm. / Cond.	Perm. / Cond.	
RR	.1 to .4	10 ac	10 acres / 10 acres	400 / 400	400 / 400	200 / 200	50 / 50	45
		5.0 ac.	5 acres / 5 acres	250 / 250	250 / 250	200 / 200	50 / 50	45
		2.5 ac	2.5 acres / 2.5 acres	200 / 200	200 / 200	200 / 200	50 / 50	45
RE	.5 to 3.0	2 ac.	2 acres / 2 acres	175 / 175	175 / 175	200 / 200	50 / 50	45
		1 ac.	1 acre / 1 acre	150 / 150	150 / 150	200 / 200	50 / 50	45
		20	20,000 / 1 acre	100 / 150	110 / 150	100 / 100	30 / 30	25
		15	15,000 / 30,000	80 / 130	100 / 130	100 / 100	30 / 30	25
		12	12,000 / 24,000	80 / 100	90 / 100	100 / 100	25 / 25	20
		10	10,000 / 20,000	80 / 100	90 / 100	100 / 100	25 / 25	20
RL	3.1-5.0	10	10,000 / 20,000	80 / 100	90 / 100	100 / 100	25 / 25	20
		8	8,000 / 16,000	70 / 100	80 / 100	100 / 100	20 / 20	15
		6	6,000 / 12,000	60 / 100	70 / 100	95 / 100	20 / 20	15
		5	5,000 / 12,000	50 / 100	60 / 100	90 / 100	20 / 20	15
RLM	5.1 - 8.0	4.5	4,500 / 12,000	45 / 100	55 / 100	70 / 100	20 / 20	15
		3.6	3,600 / 12,000	40 / 100	45 / 100	60 / 100	15 / 20	10
		C (clustered)	10 acres / 12,000 (project area)	400 / 400	400 / 400	400 / 400	15 / 20	10 (to public street)
RM	8.1-14.0	n/a	7.5 acres / 12,000 (project area)	300 / 300	300 / 300	100 / 100	20 / 20	15
RH	14.1-20.0	n/a	5 acres / 12,000 (project area)	250 / 200	250 / 200	250 / 250	20 / 20	n/a
MHP	6.0-10.0	n/a	10 acres	400	400	400	20	n/a

RR: Rural Residential
RE: Residential Estates
RL: Residential Low Density
RLM: Residential Low Medium Density

RLM-C: Residential Low Medium Density-Clustered Units
RM: Residential Medium Density
RH: Residential High Density
MHP: Manufactured Home Park, Including Mobile Homes

Min. Side Yard (ft.) (See Note 3)	Min. Side Yard - Street Side (ft.) (See Note 4)	Min. Rear Yard (ft.) (See Note 5)	Min. Site Frontage on a Public Street (ft.) (See Note 6)	Min. Setback Adjoining a Freeway, Arterial, or Railroad ROW (ft.) (See Note 7)	Min. Distance Between Structures (ft.) (See Note 8)	Max. Site Coverage (%)	Max. Building Height (ft.)	Alternate Housing Configuration Requirement (See Note 9)
Perm./Cond.	Perm./Cond.	Perm./Cond.	Perm./Cond.	Perm./Cond.	Perm./Cond.	Perm./Cond.	Perm./Cond.	
30 / 30	30 / 30	40 / 40	100 / 100	40 / 40	10 / 10	5 / 5	40 / 40	n/a
25 / 25	30 / 30	40 / 40	100 / 100	40 / 40	10 / 10	5 / 5	40 / 40	n/a
20 / 20	30 / 30	40 / 40	100 / 100	40 / 40	10 / 10	10 / 10	40 / 40	n/a
20 / 20	30 / 30	40 / 40	100 / 100	40 / 40	10 / 10	10 / 25	40 / 40	n/a
20 / 20	25 / 25	40 / 40	100 / 100	40 / 40	10 / 10	25 / 25	40 / 40	n/a
20 / 30	20 / 25	20 / 20	40 / 40	40 / 40	10 / 10	35 / 25	40 / 40	n/a
15 / 25	20 / 25	20 / 20	40 / 40	40 / 40	10 / 10	40 / 25	40 / 40	n/a
10 & 15 / 20	15 / 20	20 / 20	40 / 40	40 / 40	10 / 10	40 / 25	40 / 40	n/a
10 / 20	15 / 20	20 / 20	40 / 40	40 / 40	10 / 10	40 / 25	30 / 30	n/a
10 / 20	15 / 20	20 / 20	40 / 40	40 / 40	10 / 10	40 / 25	30 / 30	n/a
10 & 5 / 20	15 / 20	20 / 20	40 / 40	40 / 40	10 / 10	40 / 25	30 / 30	n/a
10 & 5 / 20	15 / 20	20 / 20	40 / 40	40 / 40	10 / 10	40 / 25	30 / 30	n/a
10 & 5 / 20	15 / 20	20 / 20	40 / 40	40 / 40	10 / 10	40 / 25	30 / 30	25 %
10 & 5 / 20 or 5 & 5 / 20 or 0 & 12 / 20	10 / 20	20 / 20	40 / 40	40 / 40	10 / 10	40 / 25	30 / 30	50 %
5 & 5 / 20 or 0 & 10 / 20	10 / 20	15 / 20	40 / 40	40 / 40	10 / 10	40 / 25	30 / 30	100 %
15 / 20	15 / 20	15 / 20	40 / 40	40 / 40	10 / 10	40 / 25	30 / 30	100 %
20	20	20 / 20	40 / 40	40	10 / 10	40 / 30	40	n/a
20	20	20 / 20	40 / 40	40	10 / 10	40 / 30	40	n/a
20	20	40	40	40	10	30	40	n/a

FOOTNOTES TO TABLE 14.074.01

(1) Lot Area:

- a. For the RLM-C, RM, & RH districts, the minimum area requirement refers to the total development area of a new project site.
- b. Lots which were in existence prior to the effective date of this ordinance are developable subject to the requirements of this chapter.

(2) Front Yard:

- a. All Districts:
 - i. Infill standards may apply; Section 14.074.080 should be reviewed for standards applicable to infill projects.
 - ii. For projects on private streets, setbacks may be established as part of the planned development process.
 - iii. Overhanging architectural features such as sills, cornices, and eaves without posts may extend into the required front yard up to 6 ft. This applies to the required front yard and does not apply to the front setback for porches.
- b. RR, RE, RL, RLM Districts:
 - i. To encourage variation in front yard setbacks, a front yard setback between 18 ft. and 22 ft. may be allowed in the RL-8, 6, 5, and 4.5 districts when the average front yard setback on the street is 20 ft.;
 - ii. In the RLM-3.6 district, the front setback of 15 ft. may be applied only to the house, and not to the garage. The minimum setback to the garage shall be 18 ft.
 - iii. For RLM-4.5 and RLM-3.6: these are minimum standards for detached structures on public streets; other project standards may be altered through a planned development if fronting on a private street.
- c. RLM-C, RM, & RH Districts - Permitted Uses:
 - i. For RLM-C, the 15 ft. setback applies to attached single family projects. Perimeter standards apply for other attached projects.
 - ii. On a site situated between sites improved with existing multi-family development, the minimum front yard shall be the average depth of the front yards of the adjoining sites, however, a depth of 10 ft. greater than the minimum front yard shall not be required;
 - iii. Where a site is situated between sites which are not improved with buildings and where sites comprising 40% of the frontage on a block are improved with buildings, the minimum front yard shall be the average of the existing front yards on the block, however, a depth of 10 ft. greater than the minimum shall not be required;
 - iv. To promote variation in front yard setbacks, when a multi-family development includes two or more adjoining structures, the decision-maker may require variation in front yard depths, provided that the average front yard depth shall be not less than the minimum depth required.

(3) Side Yards:

- a. All Districts:
 - i. Infill standards may apply; Section 14.074.080 should be reviewed for standards applicable to infill projects.
 - ii. For projects on private streets, setbacks may be established as part of the planned development process.
 - iii. Architectural features such as sills, cornices, and eaves may extend into the required side yard or a space between structures up to 2 ft. Chimneys may extend into the required side yard or a space between structures up to 2.5 ft.
- b. RR, RE, RL, RLM Districts:
 - i. In the RLM-4.5 and RLM-3.6 districts, the 0 & 12 ft. and 0 & 10 ft. setbacks refer to zero lot lines. Two zero lot line setbacks may not abut each other.
 - ii. Where a two-story portion of a structure or a portion of a structure exceeding 15 ft. in height is located closer than 15 ft. to a side property line, the minimum building setback shall be no less than 10 ft.
 - iii. In the RLM-3.6 district, a side yard abutting any two story portion of the structure may be 7 ft.
 - iv. For subdivisions approved prior to enactment of this ordinance, an existing dwelling may be extended along the existing legally established setback line.
 - v. In the RLM-4.5 district, for dwellings with 5 & 5 ft. setbacks, the following special standards apply: dwellings must be single story, and have a recessed garage located a minimum of 5 feet behind the front line of the habitable portion of the unit which contains the front entry to the dwelling and a covered porch.
 - vi. In the RLM district, garages located in the rear portion of the yard may be zero lot line.
- c. RLM-C, RM, & RH Districts - Permitted Uses:
 - i. For existing lots of record established prior to enactment of this ordinance with a total area of 1 acre or less, the minimum side yard shall be 5 ft., subject to the following adjustments:
 - a. An additional 5 ft. shall be added for each story above the first story.
 - b. A side yard providing access to more than 1 dwelling unit shall be a minimum of 15 ft.

(4) Side Yard/Street Side:

- a. All Districts:
 - i. Infill standards may apply; Section 14.074.080 should be reviewed for standards applicable to infill projects.
 - ii. For projects on private streets, setbacks may be established as part of the planned development process.
- b. RLM-C, RM, & RH Districts - Permitted Uses:
 - i. For existing lots of record established prior to enactment of this ordinance with a total area of 1 acre or less, the minimum side yard shall be 10 ft.

(5) Rear Yard:

- a. All Districts:
 - i. Infill standards may apply; Section 14.074.080 should be reviewed for standards applicable to infill projects.
- b. RR, RE, RL, RLM Districts:
 - i. On a reversed corner lot, the minimum rear yard may be not less than the minimum side yard for the district, provided that the side yard adjoining the street shall be not less than the required front yard of the adjoining key lot.
 - ii. In the RLM-4.5 district, the required rear yard may be reduced to 15 ft. if an average setback of 20 ft. is provided.
 - iii. Architectural features such as sills, chimneys, cornices, and eaves may extend into the required rear yard up to 6 ft.
 - iv. In the RLM District, garages located in the rear portion of the yard may be zero lot line.
 - v. If the garage is located in the rear yard as an alternative configuration design, the rear yard setback and coverage standards may be adjusted through a planned development.
 - vi. Attached, unenclosed patio covers and solarium rooms may be located in the required rear yard provided that they shall be at least 10 ft. from the rear property line, and that the area of the structure in the required rear yard shall be included as part of the rear yard coverage regulations. Enclosed solarium rooms shall be entirely enclosed in clear material such as glass or plexiglass except for one side attached to the main structure.
- c. RLM-C, RM, & RH Districts - Permitted Uses:
 - i. For existing lots of record established prior to the enactment of this ordinance with a total area of 1 acre or less, the following shall apply:
 - a. The rear yard may be reduced to not less than 15 ft. provided one side yard is not less than 25 ft.
 - b. One foot shall be added to the rear yard for each 2 ft. of ht above the lowest 15 ft.
 - c. On a reversed corner lot, the minimum rear yard may be not less than the minimum side yard for the district, provided that the side yard adjoining the street shall be not less than the required front yard of the adjoining key lot.

(6) Site Frontage on a Public Street:

- a. More than one connection to the public street may be required.

(7) Setback for Sites Adjoining a Freeway, Arterial, or Railroad Right-of-Way:

- a. Infill standards may apply; Section 14.074.080 should be reviewed for standards applicable to infill projects.
- b. Setback shall be 40 ft. from property line or 50 ft. from face of curb on an arterial street.
- c. Alternate standards apply for accessory structures. See Section 14.074.140 of this chapter.

(8) Distance Between Structures:

- a. Infill standards may apply; Section 14.074.080 should be reviewed for standards applicable to infill projects.
- b. The minimum distance for structures means either the distance between 2 structures meant for human habitation, or the distance between a structure meant for human habitation and another structure.

(9) Alternate housing configuration requirement in accordance with Section 14.074.090.